

GLOUCESTER POINT/HAYES VILLAGE DEVELOPMENT AREA PLAN

COMPREHENSIVE PLAN AMENDMENT



Gloucester Point/Hayes Village Development Area Comprehensive Plan Amendment Table of Contents

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Gloucester Point/Hayes Village Development Area

Comprehensive Plan Amendment

Executive Summary

Purpose

The Gloucester Point/Hayes Village Development Area amendment to the Comprehensive Plan began as a way to address concerns from citizens and merchants regarding the impacts of Route 17 construction and congestion on the economic viability and quality of life in the southern portion of the County. Based on the requirements in [Section 15.2-2223.1](#) of the State Code which requires localities to consider their population growth over the next 10-20 years and modify the Comprehensive Plan to identify areas the locality feels are appropriate to accommodate this growth, the County chose to focus on this area to develop a plan to better coordinate land use and transportation. The project expanded due to significant input from citizens throughout the planning process to become a community vision for the southern portion of the County. Once adopted, this plan will assist the Planning Commission and Board of Supervisors with decisions on future growth in the area.

An initial survey focused on residents and businesses in the area and nearly 300 responses were submitted sharing their thoughts and ideas. In addition a large “kick-off” meeting was held in November 2010 to provide information on the study being initiated and to obtain additional input from community members. The process was further guided by the work of a stakeholder advisory committee appointed by the County Administrator, based on direction from the Board of Supervisors. More information on the amendment’s outreach efforts is located in Appendix D.

Goals

Based on the feedback and participation from stakeholders, the work creating this amendment focused on the goals listed on this page. These goals capture the community vision and exceed the prescribed vision of the State. More information on the goals for this amendment can be found in Section IV.

Location

In Gloucester, both the central area of the county, known as Gloucester Courthouse, as well as the southern portion, demonstrate village characteristics. This is illustrated in the current Comprehensive Plan’s Future Land Use Plan for the County which designates each of these areas as “Village”. Both Villages are also located in the Development District. This portion of the County has been established as the area most

Goal 1

Engage community members in all phases of planning for the project area in order to incorporate the community’s long-term vision.

Goal 2

Ensure the area is established at a scale that is appropriate based on anticipated population and employment growth.

Goal 3

Preserve and reinvest in established residential neighborhoods.

Goal 4

Create efficient transportation options which promote connectivity between internal road and pedestrian networks and reduce dependence on the principle arterial corridor of Route 17.

Goal 5

Provide a safe, functional pedestrian environment to allow non-drivers to safely access their destinations within the project area.

Goal 6

Allow for mixed use and mixed density uses where appropriate.

Goal 7

Retain existing economic vitality and provide for progression of new economic opportunities.

suitable for new population growth. These characteristics make both locations excellent candidates for additional planning for the type of growth to achieve the mix of uses, densities, and connectivity envisioned by the County. While this document focuses on the Gloucester Point/Hayes portion of Gloucester as demonstrated on Map 1, future planning efforts will center on achieving this in the Gloucester Courthouse area as well.

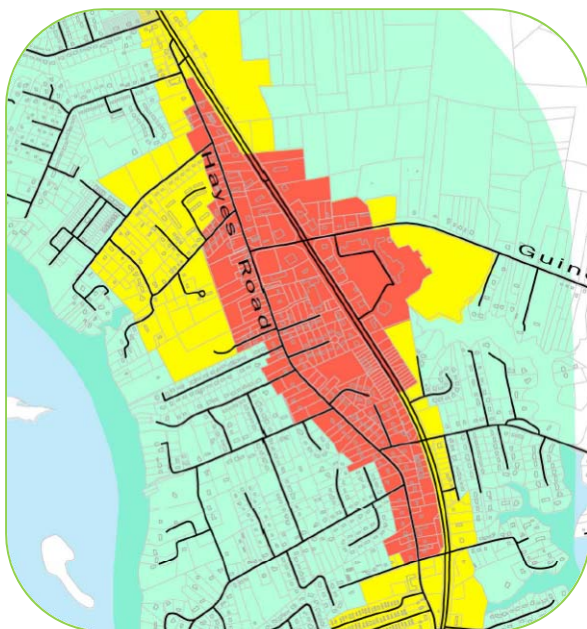
The most recent planning effort that exclusively focused on the southern portion of Gloucester County took place in the 1990's. A citizen based effort to consider the future of the area, more closely defined as the Gloucester Point Census Designated Place (CDP), resulted in the Gloucester Point Plan. Many of the concepts discussed in this amendment echo the sentiment of the Gloucester Point Plan, but renew the Plan's vision based on current concerns and community needs.

Three main sections within the CDP are identified in this plan as areas that can benefit from additional planning efforts and new implementation tools in order to achieve the village characteristics envisioned by the community: Crewe Road Area, Hayes Road Area, and Greate Road Area.

Size

Considering Gloucester's residential and commercial growth for the next 10-20 years involves utilizing Census population data and as well as projections and estimates. The Gloucester Point/Hayes area of the County was this Plan's focus, however, Gloucester Courthouse has long been an area where mixed uses and mixed density fit well and therefore is also being considered as an additional Village Development Area to serve our community. When reviewing the growth projections compiled for Gloucester by the Virginia Employment Commission (VEC), a range of acreage was calculated to serve our growth needs countywide. The available vacant lands identified in the Village Development Areas will be able to accommodate the range of anticipated growth over the next 10 to 20 years.

Land Use Plan



A map, known as the Future Land Use Plan, provides a visual representation of the areas that were studied in the Gloucester Point/Hayes area. Based on the analysis of the growth and land area desired for the Plan, as well as the consideration of the existing land use, this map outlines areas shown as: Core, Transitional, and Conventional. The Core area already contains most of the community's non-residential uses such as dining, shopping, and offices. The Transitional area contains some denser residential uses such as duplexes, townhomes and apartments. Single family homes with a few townhomes or condominiums make up the area shown as Conventional, but it remains in close proximity to the Core and Transitional areas. Details about the characteristics of each area shown on the map can be found in Section V.



Benefits

The benefits associated with the study and future implementation of the Gloucester Point/Hayes Village Development Area Plan are wide reaching. They will affect those living and working in this portion of the County, as well as those commuting to other areas along the Route 17 corridor. By providing increased connectivity for local traffic, pedestrians and cyclists, fewer cars will be utilizing Route 17. Local traffic will have more options to satisfy their travel demands and see decreases in travel time. The

many transportation benefits are explored in detail in Section VI and Appendix C, however, in addition to transportation, other benefits are realized through implementation of this Plan. These include increased economic viability in commercial areas, neighborhoods that are more walkable and less dangerous for children and other non-drivers, more choices for housing options to serve various housing needs, better planned development that emphasizes the community's needs and desires, and other quality of life improvements discussed further in Section VI.

Closing

This document represents a long-term vision based on community feedback as well as the analysis of data. Implementation of this vision will occur over time on an incremental basis, similar to piecing together a puzzle. As with any good planning tool, it is not intended to be static over the next 20 years; instead it will be reviewed and revised when necessary according to the desires of the community.

Gloucester Point/Hayes Village Development Area Plan

I. Background

The shape of American towns and cities has changed over time, with a major shift in American planning coming in the late 1940s, when suburbs as we now know them today were developed. These new real estate projects placed new homes outside of existing towns and village centers, where they could only be accessed by car, rather than close to, or within, existing areas where walking or bicycling to destinations was possible. The new suburbs were also largely made up of only one type and style of home, rather than the mix of housing and sizes common in classic villages.

New patterns of residential growth separated uses such that homes, shops, offices and, more importantly, jobs, were in different locations and necessitated destination generated driving patterns. This separation of uses was in contrast to the mixed-use character of places that relied more on walking, biking and public transportation to reach the local shop or one's job. In recent years, large lot subdivisions, more removed from major transportation corridors and public water and sewer, have been the dominant pattern of residential growth in Gloucester County.

While homes on larger lots have certain benefits, the cost of transportation and commuting patterns have had adverse effects over time. In Gloucester County during the 1980's, five acre lot subdivisions became the standard. As a result more land was consumed and new infrastructure was required to meet the rising population demand. This included new schools, utilities, and increases in number of deputies for public safety. Besides capital demands, this trend created longer commutes between home, work, shopping and leisure activities, all of which generated more traffic, road construction and maintenance to handle the demands of the suburban "way of life."

As a way to address some of the effects of sprawl and strip development, particularly on traffic and transportation, new legislation was adopted in 2007 as Virginia Section [15.2-2223.1](#) of the Code of Virginia, requiring certain localities experiencing more rapid growth to amend their Comprehensive Plans to incorporate one or more "Urban Development Areas" or UDAs. A UDA is more closely defined as an area designated by a locality that is appropriate for higher density development due to its proximity to transportation facilities, as well as public water and sewer infrastructure. Based on the language in State Code, UDAs can also be designated for infill or redevelopment. These areas must be sized to meet projected residential and commercial growth in a locality for the next 10 years, but not more than 20 years into the future. In addition, the Code indicates that federal, state and local transportation, utility, economic development, and other public funding should, to the extent possible, be directed to these locations in the County.

Minimum densities for housing types and non-residential uses are incorporated into the guidance provided from the State. These include: *four single family residences per acre, six townhouses per acre, or twelve apartments or condominium units per acre; and a floor area ratio (FAR) of at least 0.4 for commercial development* or some combination thereof. In order to achieve these minimum densities features such as those listed below should be incorporated into site design:

- (i) pedestrian-friendly road design

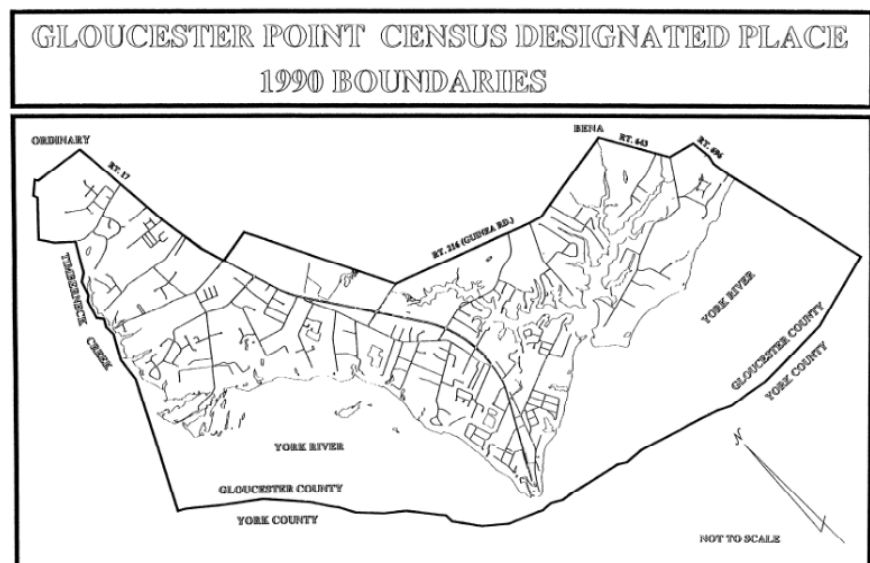
- (ii) interconnection of new local streets with existing local streets and roads
- (iii) connectivity of road and pedestrian networks
- (iv) preservation of natural areas
- (v) mixed-use neighborhoods, including mixed housing types, with affordable housing to meet the projected family income distributions of future residential growth
- (vi) reduction of front and side yard building setbacks
- (vii) reduction of street widths and turning radii at subdivision intersections

This list represents principles associated with Traditional Neighborhood Development (TND), which is also known as New Urbanism. Gloucester already has several projects that incorporate some of these principles into their design. Many were obtained by rezoning the properties in order to make the mix of uses and/or densities obtainable, however, the text in this document provides information on how other properties in the Gloucester Point/Hayes area of the County can develop in a way that represents the community's vision and accommodates 10-20 years of population and business growth. By doing so, as this growth takes place over time, individual projects can link together in a way that serves those who live, work, and visit the southern portion of Gloucester.

II. Location: Why Gloucester Point/Hayes?

In Gloucester County, both the central area of the county, known as Gloucester Courthouse, as well as the southern portion around Gloucester Point, demonstrate village characteristics. This is illustrated in the Comprehensive Plan's Future Land Use Plan for the County which designates each of these areas as "Village". Both Villages are also located in the Development District. This portion of the County has been established as the area most suitable for new population growth. These characteristics make both locations excellent candidates for additional planning for the type of growth necessary to achieve the mix of uses, densities, and connectivity envisioned by the County. While this document focuses on the Gloucester Point/Hayes portion of Gloucester as demonstrated on map __, future planning efforts will center on expanding Traditional Neighborhood Design principles in the Gloucester Courthouse area as well.

The most recent planning effort that exclusively focused on the southern portion of Gloucester County took place in the 1990's. A citizen based effort to consider the future of the area, more closely defined as the Gloucester Point Census Designated Place (CDP), resulted in the [Gloucester Point Plan](#). Map __ shows the boundaries that were utilized in that planning effort. Many of the concepts discussed in this planning document echo the sentiment of the Gloucester



Point Plan, but renew the Plan's vision based on current concerns and needs.

The work represented on the following pages of this amendment began with the Gloucester Point Plan and further analyzed the area within it. After reviewing existing conditions and listening to concerns from area citizens and businesses, three main sections within the CDP were identified for inclusion in the Gloucester Point/Hayes Village Development Area: Crewe Road Area, Hayes/Guinea Road Area, and Greate Road Area. These areas were selected based on existing conditions such as shopping, dining, parks, schools, water and sewer availability, and transportation connectivity options which allow people to enjoy these amenities in close proximity to their homes without the need to travel by car on Route 17. Each of these areas offers their own unique features, however, they are all well suited for planning and implementation efforts such as those described this document. Through thoughtful decision making and review of new development and redevelopment proposals, the County can retain the existing viability of the community as well as foster opportunities to improve it. In the future, additional sub-area planning for these designations or master plans for individual projects can further help the County focus on the specific needs of each segment.

Some of the attributes that make Gloucester Point and Hayes the top choice for village-scale density and development also represent challenges. While Route 17 serves as an important area of commercial services for local residents, it is also a major regional commuter route with very high traffic volume. Route 17 effectively divides Gloucester Point. Development that successfully connects residents and businesses across Route 17 will be extremely difficult. The type of village-scaled development that is associated with Traditional Neighborhood Design, is primarily built around small-scale street systems that serve pedestrians and cars at low volumes and speeds, not major arterial highways. For this reason, TND land uses that directly address the street, such as residential uses with small front yards and front porches, shop-front retail and service uses, cafes, and small parks, are better suited to narrower and quieter parallel or perpendicular streets than to the immediate Route 17 corridor.

A second challenge to new, village-scaled development is the existing pattern of development in the Gloucester Point area. The Point is largely made up of auto-oriented shopping centers along Route 17, with suburban-scaled single family housing behind. The area is largely built out, with only small, scattered sites undeveloped. Because of this pattern, changes to the development character of Gloucester Point, from auto-oriented to village-like, will only happen over time, with small new developments, infill lot development, and redevelopment of existing sites and structures as they age or no longer meet market demands.

Consideration of how to outline the Gloucester Point/Hayes Village Development Area should focus on areas within Gloucester Point where village-scaled new development can be centered away from the busy Route 17 corridor; yet remain visible and accessible from the corridor. Areas of existing importance, such as shopping and other activity centers, should be taken into account. Use of signage will be important for assistance in wayfinding. Areas of infill and redevelopment potential should also be identified. Taking these factors into account, staff, consultants, and the UDA steering committee have identified Urban Development Areas as depicted on the following map.

This plan establishes core UDA zones – areas where the most intense new development should be permitted in the future – at three of the most important nodes in the Gloucester Point area.

o Greate Road: future development along Greate Road has the potential to take advantage of the activity and employment of the Virginia Institute of Marine Science as well as the recreational activity associated with the Gloucester Point Beach and boat ramp. New businesses and residences might cater to this, and surrounding, areas, while taking advantage of an area that is somewhat isolated from the traffic of Route 17.

o Hayes Road & Guinea Road: as a major intersection, and a center of existing shopping in the area, this crossroads will continue to be an important center for Gloucester Point. New development at this location can set a village-scale, rather than strip shopping center, pattern for a larger area. Central to further development of this area is access to Hayes Road, a long parallel to Route 17 that has not developed in the same automobile-centric fashion, and which may represent the area's Main Street in the future. The presence of the Gloucester Point library, a movie theater, post office and several restaurants and retail establishments makes this intersection a prime destination for local activity.

o Crewe Road: located at the northern limit of the Gloucester Point/Hayes Village Development Area. With existing shopping options and a public school, as well as several undeveloped sites, new development in this area can add new homes and businesses in a walkable pattern and take advantage of the existing connectivity of the existing road network. By focusing on the Crewe Road frontage, development in this area can avoid Route 17's traffic, while remaining visible and accessible.

At each of these important core areas, this amendment proposes TND development and redevelopment that combines commercial and residential uses in a compact, walkable pattern. These mixed-use cores should be surrounded by additional transitional areas, where compact neighborhoods, with some small commercial uses, support the businesses in the core. Beyond these core and transitional areas in the Gloucester Point and Hayes portions of the County, are many established neighborhoods that will contribute to the functional development within the Core and Transitional areas.

III. Goals

Future development in the Gloucester Point/Hayes area will be guided by this and other sections of the Comprehensive Plan, in addition to the ordinances that are written to support the community's vision. The 1995 Gloucester Point Plan provides additional goals specific to the Gloucester Point area and are a common thread in the current community. Some of these include:

- *"Concentrate new development in appropriate locations by encouraging more efficient site design and the sharing of access, parking, and utilities."*
- *"Authorize higher densities outside the 100 year floodplain where public water and sewer service, open space, and other amenities are incorporated in site design proposals."*
- *"In cooperation with VDOT, plan for improved pedestrian walkways and bicycle paths within public rights-of-way whenever possible."*

The Gloucester Point plan goes on to set future land use goals for this area centered on the idea of a village scale. This plan's suggestion that future development within the Gloucester Point CDP should be

based on neighborhood and mixed use centers, and feature parks and pedestrian amenities, rather than a continuation of strip commercial development.

These goals are very much in line with ones established in conjunction with this Comprehensive Plan amendment. Using the feedback received from the public at the “kick off” meeting in November 2010 as well as the responses to the survey that nearly 300 area citizens completed, the following goals were established and serve as guidance for the Gloucester Point/Hayes Village Development Area:

Engage community members in all phases of planning for the study area in order to incorporate the community’s long-term vision.

A significant amount of outreach was held in conjunction with this planning effort. Community meetings, online/phone surveys, and formation of a steering committee with staff and local stakeholders were a start. Work will continue to keep the community engaged throughout additional steps as well as during implementation elements.

Ensure the area is established at a scale that is appropriate based on anticipated population and employment growth.

A demographic analysis was performed using the available population and employment numbers provided by the U.S Census Bureau and the Virginia Employment Commission (VEC). These numbers and the associated analysis provide the data necessary to ensure that the area selected meets the needs of our growing community.

Preserve and reinvest in established residential neighborhoods.

Existing residential neighborhoods help shape the southern portion of Gloucester County. Great care has been taken to ensure that these areas can remain as they are today and also benefit from the potential opportunities being created around them. This includes safer streets and increased pedestrian walkways to current goods and services such as the grocery stores and post offices as well as future amenities such as diverse dining and shopping options.

Create efficient transportation options which promote connectivity between internal road and pedestrian networks and reduce dependence on the principle arterial corridor of Route 17.

Ensuring that new roads are connecting to existing ones and incorporating pedestrian options will help everyone living, working, and visiting Gloucester Point and Hayes access goods and services and decrease trips on Route 17.

Provide a safe, functional pedestrian environment to allow non-drivers to safely access their destinations within the study area.

The Gloucester Point/Hayes Village Development Area must incorporate functional pedestrian options that allow non-drivers to access to their destinations such as jobs, doctor's offices, grocery stores, and even the public library. Safe crossings of intersections, especially those incorporating Route 17, are important to allow non-drivers to be a vital part of our community.

Allow for mixed use and mixed density uses where appropriate.

Mixed use development that incorporates some retail or office space with residential space is more consistent with the areas identified on the map as "Core" areas, while mixed density residential fits in the areas identified as "Transitional" areas in most cases. However, there should be a review of development proposals in order to review each for suitability with the community's vision and established County guidelines.

Retain existing economic vitality and provide for progression of new economic opportunities.

Over the years Gloucester Point and Hayes have provided many business options for County residents. The Gloucester Point/Hayes Village Development Area amendment should assist these businesses in preserving and expanding as desired. Increased opportunities for new development and redevelopment should result in improved economic vitality for those existing businesses by attracting more people to the area as a destination as well as capturing more local dollars from those choosing the convenience of shopping locally in a village setting.

IV. Land Use Recommendations

Earlier in this amendment, the three main portions in Gloucester Point and Hayes were identified as along Greate Road, Hayes/Guinea Road, and Crewe Road. At each of these important focus areas, this amendment proposes TND development and redevelopment that combines commercial and residential uses in a compact, walkable pattern constructed around an analysis of its land use areas and neighborhoods. Incorporating a mix of uses and densities into growth is very different from the separation of these considerations that has been traditionally applied to development in Gloucester. Development that uses principles of TND is set apart from typical suburban construction by its focus on compactness, its mix of land uses, and other features aimed at building meaningful neighborhoods and places rather than subdivisions full of single family detached homes and shopping centers. It is a common suburban development practice to organize land uses into separate areas for commercial use, residential use, etc. well planned TND development is, instead, organized into sections based on intensity of use and activity. For Gloucester, these are identified as the mixed-use "Core" areas as well as "Transitional". Mixed-use cores should be surrounded by additional transitional areas, where compact neighborhoods, with some small commercial uses, support the businesses in the core. Beyond these core and transitional areas in the Gloucester Point and Hayes portions of the County, are many

established neighborhoods that will contribute to the functional development within the Core and Transitional portions.

The Core Area: The Village Center

The Village Center is the geographical and functional core or “heart” of the community. Neighborhood gathering, recreation, shopping, and transportation are all focused on this core area. Traffic patterns for automobiles and pedestrians also radiate from this central location. The Village Center should be seen as the heart of the TND community in much the same way as a Main Street, important crossroads or set of downtown blocks forms the heart of older towns and villages throughout Virginia and elsewhere. While Gloucester Point and Hayes have a great deal of commercial development, this development is almost entirely made up of car-oriented strip shopping centers. The community as a whole should have a concentric relationship to the village center, with the densest development located within the village center, town homes and other attached units located nearby and detached homes farther from the center. In keeping with the overall goals for TND development, the village center should incorporate a mix of uses that include commercial, civic, residential, and recreational land uses.

Mixed Use

The village center is the primary location where commercial uses can be mixed into the community, alongside, or collocated with other land uses. The village center should allow and encourage small retail, service, and office uses, located along walkable streets within the TND core. Commercial and civic uses should be designed to serve the needs of the immediate neighborhood, including dining, as well as serve customers traveling to the village center from other communities. The typical layout of such uses places retail and service uses at the street level, with office or residential uses above.

Yards and Setbacks

Commercial uses should be located close to or immediately at the edge of the sidewalk. Because the village center is designed with pedestrians in mind, shallow setbacks allow users to access the commercial uses quickly and safely, without crossing large parking lots. This arrangement also creates a sense of enclosure on village center streets that adds to the appearance and vitality of the village center.

Within this arrangement, larger setbacks are possible, but should be reserved for certain cases. One such case is for important buildings such as museums, libraries, or other civic spaces where a plaza or other appropriate open space can add importance and interest to the building and surrounding streetscape. In another case, dining establishments can use a deeper setback to include outdoor dining space that will add vitality to the street. In both cases, these deeper setbacks should be used carefully and sparingly, with the overall pattern of the village center in mind.

Civic spaces

In addition to commercial and residential uses, the village core should include at least one civic space as a community gathering area for residents, or for community events. This civic space may take the form of a landscaped park, hardscaped plaza, or civic building such as a library, church, or government center. Such a civic space can provide an important relief from the density of the town center, and should be placed near the center of the village, or in a place of importance, such as at the termination of a street.

Parking

The village center is intended as a commercial area that caters more to pedestrians than do typical suburban shopping centers. For this reason, parking in the village center should be carefully planned to allow access to village center uses without harming the feel or function of the area.

All streets within the village center and within the TND community in general, should include on-street parking. When off-street parking is necessary, parking lots should be implemented behind buildings. When street-front parking cannot be avoided, parking lots should be screened from view with walls, fences, or landscaping. To facilitate these parking arrangements, shared parking strategies, public parking areas, and structured parking should be considered. Flexibility needs to be incorporated into ordinances that typically focus on conventional development that separates uses.

Transitional Area

The Transitional Area is meant to divide the active and highly commercial Core Area from quieter residential areas. It is as a bridge between the activity and high density of the core, and quieter, less dense residential areas. The Transitional Area will be primarily residential, but made up of more intensive residential uses such as apartments, condominiums, and town homes. The Transitional Area will also include a mix of commercial uses, but at a lower commercial density than in the Core Area. It serves as the ideal location for small shops, restaurants, or offices that primarily serve the residences around them. Development parameters for the Transitional Area should seek to bridge those used in Core and residential areas, including minimums and maximums established for building height, front and side yards, and other parameters.

Live-Work Units

The Transitional Area is a good place to allow live-work units, a type of commercial and residential use in one, designed to have a ground floor shop, with an apartment or townhouse on floors above for the shop owner or operator. These units are typically associated with small startup businesses that are ideal for the TND scale and transitional area.

Lot Size

Residential lots located in the Transitional Area are necessarily smaller than lots in many typical suburban developments. Smaller lots put houses closer to each other, and closer to the village center, in order to preserve walkability in a more compact community. The result is a given number of residential lots occupying less total acreage than with typical suburban construction. The TND community should include a wide variety of lot and house types at a variety of sizes, from small town house lots near the village center, to small single family detached lots, and larger estate lots away from the core. Final lot sizes should be based on careful master planning, as well as market demand for residential uses.

Yards and Setbacks

In order to achieve density and walkability goals, residences in this type of development are built closer to the street, and closer to each other. Within the narrow, pedestrian oriented streets of a TND, an appropriate residential front yard may be as little as 10 or 15 feet. TND communities also often have a maximum building setback (for example, 20 feet) in order to allow variation, while also maintaining a regular and ordered appearance within the neighborhood.

Rear garages and parking areas

As with parking in the village center, residential parking areas and garages should be established in a way that make vehicle access convenient, but without negatively impacting pedestrian access. In most cases, communities that incorporate TND principles are designed with mid-block alleys, allowing access to garages at the rear of residential lots. Other options for front access lots include those that share driveways between two houses, and side yard driveways to access rear lot garages.

Lot mix

In order to create the type of successful neighborhoods found in many established towns and main streets, projects must maintain a careful balance between housing types and sizes. In typical suburban neighborhoods where only a few (or only one) home type or design is allowed, the result is a sense of sameness seen in many conventional developments. While this mix will necessarily be influenced by market demand, various house types (single family, town home, and condo) and sizes should be mixed together generously to give the TND neighborhood a feeling of originality and variety.

Other Land Use Considerations

Land uses that may still be important to the overall function of a community should not be completely ruled out when considering the incorporation of the TND principles associated with the Core and Transitional Areas. Certain economic development activities that are not traditionally included may include light manufacturing uses, big-box development, or even agriculture at certain scales. While these uses are not traditionally compatible village scale development, they may be incorporated into a larger plan as a thoughtfully planned exception, and should not be excluded outright without careful consideration. Land uses at a proper scale that attract people to the area and generate a sense of community and vitality typically found in a village should be encouraged. Implementation that allows for flexibility of design while still insuring that individual proposals contribute to the overall village concept for that designated area is important to achieving the community's long term vision over time.

In addition, properties located outside of the mapped Core and Transitional Areas may be well-suited for consideration for mixed-use and/or mixed density. As time passes and pieces of this plan are put into place, property owners in these locations outside the current boundaries of the Gloucester Point/Hayes Village Development Area may seek these designations through rezoning applications or ask the County to reflect on including their sites in future planning efforts. In the meantime, these areas should provide for connectivity for bicycle, pedestrian and other transportation modes that would allow for longer distance travel between the village areas identified.

V. Benefits

When planning for development to occur over time as outlined by the community's vision, many benefits are realized by numerous stakeholders. Some are realized in cost savings, such as reduced infrastructure costs for roads and utilities. Other benefits cannot be assigned a dollar amount, such as an increased sense of community, more recreation opportunities, and safer streets for children, seniors, and other non-drivers. The following sections try to identify some of the benefits that can be realized by those living, working, and/or visiting the Gloucester Point/Hayes Village Development Area.

Quality of Life

Quality of life is difficult to measure but is apparent in more ways than one. One overall objective of the Gloucester Point/Hayes Village Development Area is to create roads and neighborhoods that are diversified, aesthetic, and healthful. TND communities might be described as healthy for two principal reasons. First, these places tend to have much greater levels of neighborhood social interaction, with residents experiencing a sense of belonging to a community. In effect, they are more "livable". This community vitality promotes the development and serves to attract new residents and businesses, as well as to further promote this pattern for future developments. Second, the individual residents of a TND may see health benefits from walking or bicycling within the community in ways that aren't seen in conventional suburbs due to the safety concerns of walking or cycling where appropriate facilities are not present, or where greater travel distances and high speed traffic discourage anything but automobile travel. The incorporation of safety elements, such as well-marked crosswalks across Route 17 at key locations will help unite the currently bisected portions of Gloucester Point and allow people to safely travel to area parks or just visit friends, without the concern of peak traffic and potential congestion.

Economic

Vitality

By expanding the options for commercial uses, vacant buildings will begin to fill and new or redeveloped opportunities for various jobs can be created. Current businesses can thrive even with the newly installed concrete median near the Coleman Bridge with increased access for pedestrians. Increased convenience resulting from easier accessibility of goods and services close to home should generate additional customer loyalty to local businesses. As a sense of community and place grows in an area, local merchants should also benefit from new destination traffic as well as capturing more of the commuting traffic that may choose to stop and shop on this side of the Coleman Bridge rather than traveling across to the Peninsula.

Safer Streets

The inherently slower speed of locally connected streets, when compared to highways and suburban collector roads means greater safety for drivers and pedestrians alike. With cars slowed to reasonable in-town speeds, pedestrians are more easily seen by drivers and have more time to cross streets to reach their destinations. Slower automotive speeds also increase safety for drivers, with damage and injury reduced when collisions do occur. A study by Swift and Associates determined that the safest streets were those built 24 feet wide. Despite this, in years past many residential streets built to VDOT standards require streets to be built at a minimum of 36 feet wide. TND style development and the concept of "complete streets" enhance safety for all users of public rights of way.

Improved Emergency Response

Another safety issue presented by sprawling and disconnected suburban streets is their effect on emergency response by fire and rescue services. Sprawling suburbs mean longer distances to travel

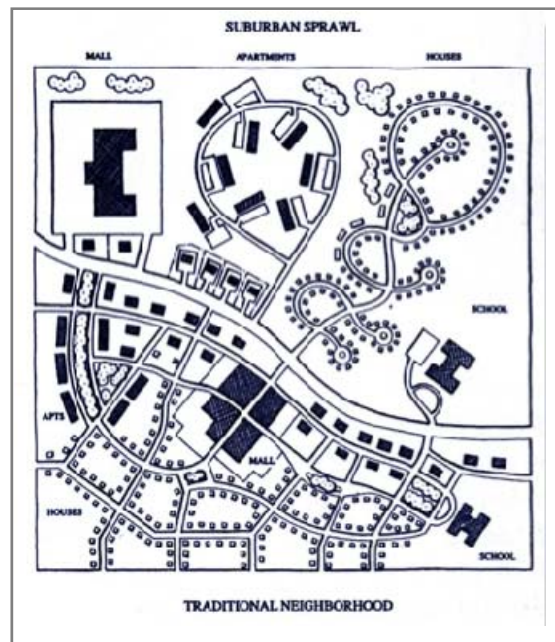
between fire and rescue facilities and some homes, while the disconnected nature of cul-de-sac streets means traveling indirect routes to answer calls. The proximity and connectivity in more densely developed areas have the potential to shorten emergency response times in more instances.

Less Time on the Road

Density and mix of uses puts daily shopping, employment, and entertainment needs within much closer proximity to residential areas. This proximity reduces time spent commuting and traveling to purchase goods and services resulting in increased residents' free time.

Transportation Efficient Land Use

Traditional Neighborhood Development within the Gloucester Point/Hayes Village Development Area has the potential to make important changes to the efficiency and effectiveness of the area's existing transportation framework. As it now stands, southern Gloucester has a mix of public and private residential streets, often with ditch sections and no sidewalks. With little pedestrian infrastructure, and with large, isolated commercial land uses that do not support walking, nearly all trips must be by car. In addition, streets have limited connections, meaning most or all trips must use Route 17. Local, as well as commuter traffic, has made Route 17 a busy and congested thoroughfare, carrying over 30,000 trips per day.



Encouraging Traditional Neighborhood Development transportation patterns in Gloucester Point and Hayes can offer certain transportation benefits as this area of the County grows, including generating fewer, and shorter, car trips than the current system. The rise of Traditional Neighborhood Development as a planning and land use trend over the past 20 to 30 years is due in part to its many advantages and benefits when compared to conventional suburban planning and development patterns.

The benefits of strengthened transportation planning in the Gloucester Point/Hayes Village Development Area can extend to everyone who lives and works there, and also to local government and citizens of the larger region. Many of the benefits of TND streets can be measured in terms of increases in system capacity, greater choices to satisfy travel demands, shorter travel times, construction cost savings, and reduced maintenance. On the other hand, other equally important attributes are linked to less scientific quality of life, aesthetic, and safety factors.

Reduced Trip Generation and Internal Capture

A well planned TND street network for Gloucester Point/Hayes has a distinct advantage over the continued expansion of suburban transportation patterns. This advantage is the reduction in the overall traffic burden on a locality's regional traffic system as well as the number of vehicle trips that are generated. A main focus of TND communities is their mix of uses that combines residential, civic, institutional, and commercial uses into one project on one site, as opposed to creating an enclave or isolated development modules each serving a different use. A resident of a typical TND community

would be able to complete certain daily tasks, like grocery shopping, dropping a child off at school, or going out for a meal, without leaving the community. With tighter knit neighborhoods, the vehicle does not need to be used to satisfy each and every trip purpose. In contrast, these same tasks in other parts of the County would require leaving a residential development and driving on an arterial highway or major collector road to a commercial development. Trips by TND residents that are made without leaving the TND are called internal capture. These are trips that are shorter, more accessible, safer, and, in some cases, can be accomplished by walking.

When analyzing traffic impacts for new developments, the Virginia Department of Transportation allows for the possibility that up to 15% of all trips by TND residents will be internally captured. This means 15% less traffic placed on existing external roads, which can lead to savings in road widening, turn lanes, and signalization. However, actual case studies comparing TND to conventional suburban projects in Virginia and other states have realized even better rates of internal capture, with 25% or more of trips staying within the TND. This is examined in greater detail Appendix C. In summary, a well planned mix of uses, with residences in closer proximity to employment and shopping needs, has the potential to reduce overall traffic on Gloucester Point's major connectors by significant rates.

Increased Transportation System Efficiency

The key difference between TND street patterns and those found in conventional suburban developments is the use of an interconnected grid of streets, while most conventional subdivisions use a disconnected pattern of streets with many cul-de-sacs and several high-volume collector roads. The overall effect of the TND grid pattern is to divide neighborhood traffic between many small streets rather than concentrate it on a few large collector roads. In the Gloucester Point/Hayes Village Development Area many streets (public and private) are already in place, however, well planned future development can improve the quality and connectivity of these streets.

Traffic analyses employing commonly accepted Highway Capacity Manual calculation methods for these two types of street networks clearly reveal that a compact network of small interconnected streets has more traffic capacity than the same street area combined into large collector streets. Intersections, not travel lanes, control the capacity of a street system, and left hand turning movements represent the critical factor that impacts capacity reduction. The typical TND network reduces the loading of left hand turning movements by distributing traffic to and through a larger number of intersections, allowing the entire system to carry a greater load. The net benefit is the need for fewer travel lanes, fewer traffic signals, and fewer traffic accidents while increasing overall system effectiveness.

The overall transportation system capacity of Gloucester Point/Hayes is currently dictated by the capacity of Route 17. By adding alternative and parallel routes, overall system capacity can be increased, and traffic congestion at key points improved.

Relationship to Regional Transportation Network

An additional positive feature of grid street patterns is the potential for interconnections to adjacent developments. Conventional subdivisions are typically self-contained, having a single entrance from a major collector road. The impact of this has been witnessed elsewhere in Gloucester County. Moreover, this is common to many rapidly growing Virginia localities that have experienced increased frontage development pressure on their arterial highways but little in the way of adjoining collector and local street improvements. This means that to visit an adjacent development, a resident would have to drive

onto the arterial highway or major collector road like Route 17 and then enter the neighboring development.

Arterial highways are intended to serve regional traffic patterns, with limited access to private properties, while Route 17 currently serves both regional and local traffic, creating conflicts that are difficult to resolve. In general, regional arterial systems are not designed to function well with high levels of frontage access, but commercial traffic cannot be restricted unless local zoning laws limit access management. Some access management is currently in place, such as the Highway Corridor Overlay District and Virginia Department of Transportation's (VDOT's) regulations that went in to place in 2008 and 2009¹. However, recent actions have proved very limited in truly managing access points, even along Route 17- a principal arterial. This results in increased travel time and trip distance while further diminishing the capacity and function of the existing system. Over time, as arterial traffic increases, the trend is to add lanes to the existing system rather than building another way to access the developed commercial areas, such as recent improvements to widen the southern-most stretch of Route 17 in Gloucester Point, and this rarely fixes the problem.

Pedestrian and Non-Car Trips

The density, mix of uses, and connected streets that are key features of TNDs make it possible to navigate new development, and possibly nearby areas, without a car. New residential development should ideally be placed within a walkable distance of new and existing commercial development. While this doesn't mean that cars aren't necessary in these areas, it does mean that certain trips, for shopping, dining out, or visiting neighbors, might be short enough to consider walking or bicycling.

Transportation guidelines for areas incorporating principles of TND encourage reduced lateral clearance between the street lanes and street trees and other forms of landscaping, thereby creating a natural, and more attractive form of traffic calming. Further, with a well planned, interconnected street system, pedestrians and cyclists have the option to select safer and quicker routes based on their knowledge of real-time traffic conditions in their neighborhoods. Unfortunately, road building in Virginia in years past often fails to emphasize pedestrian improvements, as seen in recent upgrades to Route 17 in Gloucester Point. Having a plan that identifies the community's desire to provide for non-automotive modes of transportation will allow for better coordination of transportation funds to insure that the transportation needs of all users are considered.

The number of trips made without a car will vary widely depending on the features of village, as well as factors like weather. However, case studies reveal high levels of internal traffic capture, showing that among people shopping and dining in TND commercial areas, as many as 18% had traveled there on foot. This represents an 18% reduction in traffic over single-use suburbs where no walking trips are possible due to long distances and unfavorable conditions between residential and commercial areas. The challenge for future transportation improvements in the Gloucester Point/Hayes Village Development Area will be to implement consistent design standards that work in a complementary way to enhance capacity. Additional comprehensive transportation planning studies in and around the Gloucester Point/Hayes portion of the County that focus on opportunities for (a) interconnectivity, (b) intersectional capacity enhancements, (c) upgrade of existing interior streets, and (d) integration of new street systems with existing roads should be considered by the County.

¹ These regulations are currently under review based on legislation from the 2011 Session of the Virginia General Assembly.

POTENTIAL FOR INFRASTRUCTURE SAVINGS:

Narrower Streets

Traditional Neighborhood Developments are typically designed around much narrower street cross sections than those found in conventional suburban subdivisions. These narrow streets serve to slow traffic, increase safety for pedestrians, help meet the community's goals of higher density, and also to lower infrastructure costs. These narrow streets of course require less surface pavement, but also save on foundation materials, pavement depth, turns lane length, and other factors due to their lower design speed. Existing narrow streets, with ditch drainage and no sidewalks could easily be upgraded to better meet the street section guidelines presented earlier.

On-Street Parking

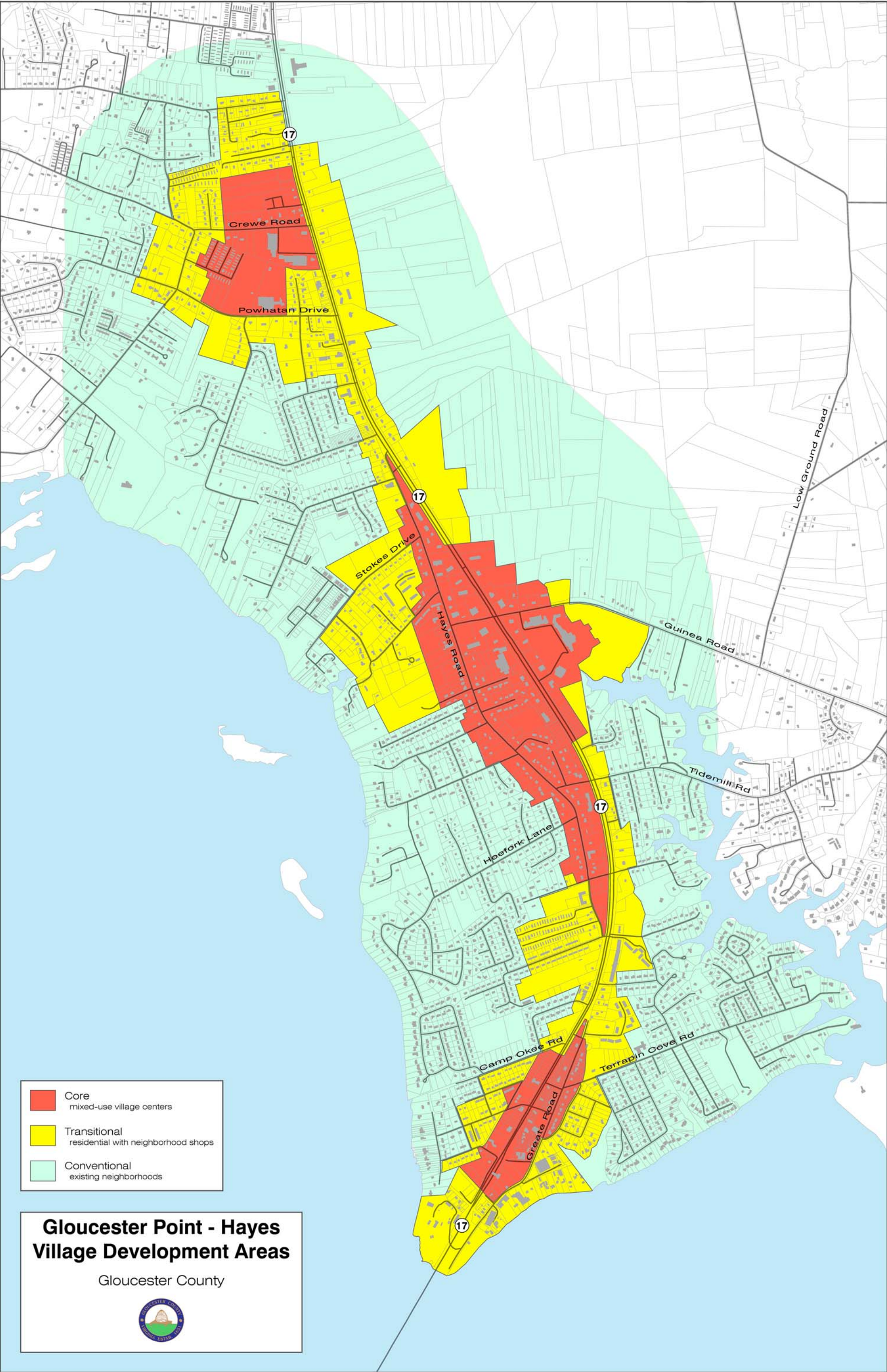
Parking can be primarily located along streets and not in large surface lots at the front of buildings as with other types of development. This means less pavement overall dedicated to parking as parking lots provide spaces and aisles, while on-street parking uses the travel lane to serve the aisle function. Many conventional residential suburbs actually are planned for on-street parking, but with ample private driveway access in front of each home, the on-street spaces are very inefficient and rarely used. Outside of cost alone, this on-street parking solution is more attractive, brings buildings closer to the street, and serves pedestrians better than suburban residential methods.

More Efficient Utility Systems

Another result of TND density is the ability to serve larger numbers of homes with shorter lengths of utility infrastructure. As with roads, more homes are served by a given length of service for sewer, water, electric, and other public services. Storm drainage systems can be incrementally reduced by virtue of less public street pavements and impervious surface. While increased density may mean a more complex infrastructure within the TND, the greatly reduced length can greatly reduce capital and maintenance costs for these services.

In addition, narrower streets allow for shorter lateral stub-outs to individual lots for public water, sewer, electricity, gas and the like. It may be necessary in many jurisdictions for VDOT, quasi-public utility agencies and local governments to rethink and revise suburban engineering standards for compact infrastructure, alignments, shared easements and rights of way, and reduced geometric requirements.

The combined effect of the instances of infrastructure savings mentioned here is to reduce the cost of building in the Gloucester Point/Hayes Village Development Area as compared to conventional suburban developments. This effect has been measured in a 2009 study for the EPA to be an infrastructure savings of between 32% and 47%. Also, while these benefits to transportation and other infrastructure and directly measurable, TND communities present additional aesthetic and quality of life benefits that, while difficult to measure, are significant to those in and around them.



Core
mixed-use village centers

Transitional
residential with neighborhood shops

Conventional
existing neighborhoods

Gloucester Point - Hayes

Village Development Areas

Gloucester County



Appendix A: TND Principles

A positive response to the long-term results of suburban sprawl over the past 50 years has been the efforts of some architects, engineers, and planners to design a new form of real estate development that draws on the features of successful cities and towns of the past. Alternately called Traditional Neighborhood Development, New Urbanism, or Neo-traditional Neighborhood Design, this form of development attempts to build mixed-use communities that function as villages rather than as disparate parts, to place uses close enough to each other to allow walking or bicycling in addition to automobile travel, and to build all of this at a convenient, attractive, and functional human scale.

TND PRINCIPLES

The following is a list of TND Neighborhood features drafted by Andres Duany, an internationally known architect and urban planner who has been instrumental in establishing TND as a modern community planning movement. Mr. Duany's list of TND neighborhood features provides a good base for understanding TND development. The neighborhoods that this list talks about are the basic building blocks of traditional neighborhoods, towns and cities. Each neighborhood is a well-defined place with its own uses, look, and feel. Any number of residential neighborhoods, transitional neighborhoods, or core neighborhoods, where commercial uses are located, can combine to make up a larger TND community.

Future development in Gloucester Point, whether new development, infill, or redevelopment, should strive to achieve these characteristics. While it may not be possible for a given neighborhood to exhibit all of these features, each of the items in this list builds upon the others to create a meaningful place that is viewed more as a village and community than simply as a housing development. It is no coincidence that these features are also those exhibited by the most successful and desirable historic towns and villages in Virginia.

1. The Neighborhood has a discernible center. This is often a square or green, and sometimes a busy or memorable street intersection. A transit stop would be located at this center.
2. Most of the dwellings are within a five-minute walk of the center. This distance averages one-quarter of a mile.
3. There is a variety of dwelling types within the Neighborhood. These usually take the form of houses, rowhouses, and apartments, such that younger and older

people, singles and families, the poor and the wealthy, may find places to live.

4. There are shops and offices at the edge of the Neighborhood. The shops should be sufficiently varied to supply the weekly needs of a household. A convenience store is the most important among them.
5. A small ancillary building is permitted within the backyard of each house. It may be used as one rental unit, or as a place to work.
6. There is an elementary school close enough so that most children can walk from their dwelling. This distance should not be more than one mile.
7. There are small playgrounds quite near every dwelling. This distance should not be more than one-eighth of a mile.
8. The streets within the Neighborhood are a connected network. This provides a variety of itineraries and disperses traffic congestion.
9. The streets are relatively narrow and shaded by rows of trees. This slows down the traffic, creating an environment for the pedestrian and the bicycle.
10. Buildings at the Neighborhood center are placed close to the street. This creates a strong sense of place.
11. Parking lots and garage doors rarely front the streets. Parking is relegated to the rear of the buildings, usually accessed by alleys.
12. Certain prominent sites are reserved for civic buildings. Buildings for meeting, education, religion, or culture are located at the termination of the street vistas or at the Neighborhood center.

TRANSECTS: A Comprehensive Planning Approach

Transects are an attempt to describe built and natural environments within a regional context. Born out of the New Urbanism movement of the 1980s, transects describe a geographical gradient of development areas, uses, and densities based on their proximity to the jurisdiction's core area. As a planning doctrine, transects mimic the concentric ring plan seen in many city plans. Transects organize all development along a scale centered on the existing core area; the farther out one gets from the core, the less density is allowed. From a comprehensive planning standpoint, transect areas are typically defined by and organized into six distinct planning sub-areas, ranging from sparse rural farm houses to

dense urban buildings. Each transect has specific rules and regulations governing building types, development form, densities, setbacks, and lot types. These zones are generally classified, as follows:

- o *Natural areas and preservation areas (little or no development),*
- o *Rural areas (very low residential densities associated with agricultural activities),*
- o *Neighborhood edge areas (transitional, suburban densities on the edge of town),*
- o *General neighborhood areas (medium residential densities within traditional subdivisions),*
- o *Neighborhood centers (high density residential areas within walking distance to core areas), and*
- o *Urban core areas (highest residential densities coupled with town center commercial development).*



For Gloucester County, the full spectrum of transects would encompass everything from sensitive marshlands, to rural agricultural lands, to core development areas in Gloucester

Courthouse and Gloucester Point. It is important to note that the Gloucester Point/Hayes Village Development Area planning study deals only with the more dense, urban end of the transect spectrum, while other elements of the County comprehensive plan will continue to treat other areas. While not intended to conflict with other components of the comprehensive plan, this plan amendment is intended to establish locations and guiding principles for the governance of higher density, mixed-use TND projects. This should certainly not diminish the importance and viability of transect planning as a bona fide regional growth management process for the entire County.

Appendix B: TND Design Guidelines

This section of Design Guidelines is intended to provide basic guidance on the physical and organizational elements of Traditional Neighborhood Design. While specific dimensions are given here, these should be seen as guides. Other lot, street, or utility designs can work just as well, but should strive to achieve the TND goals of the models in this section. The major parts of these guidelines are those that will most shape the character of new TND development in Gloucester County, Residential Lots, and TND Streets.

Residential Lots

The model lot types included here prescribe sizes and setbacks much like the County's existing zoning ordinance, but tailored for TND residential neighborhoods. These lots, in general, are much smaller than those found in typical suburban developments, have smaller setbacks from the street, and also include maximum setbacks to create a consistent street wall. This section includes the following six lot types:

Cottage Lot
Village Lot
Neighborhood Lot
Suburban Lot
Townhouse Lot
Live-Work Lot

Streets

The more compact forms of development included in TND and UDA planning also rely on more compact street designs. These street designs also strive to be complete streets, that is, streets that work for cars, pedestrians, and bicycles alike. In their overall layout, TND streets should aim for a grid pattern with many connections, as opposed to the cul-de-sac pattern with few connections seen in typical suburbs.

Thoroughfare
Village Street
Residential Street
One Way Street
Ally

Finally, this section includes basic guides for other TND elements, including street furniture, fences, stormwater, and utilities. As with all TND design elements, these features should be coordinated and specifically designed for the more compact nature of TND communities.

TND RESIDENTIAL LOTS

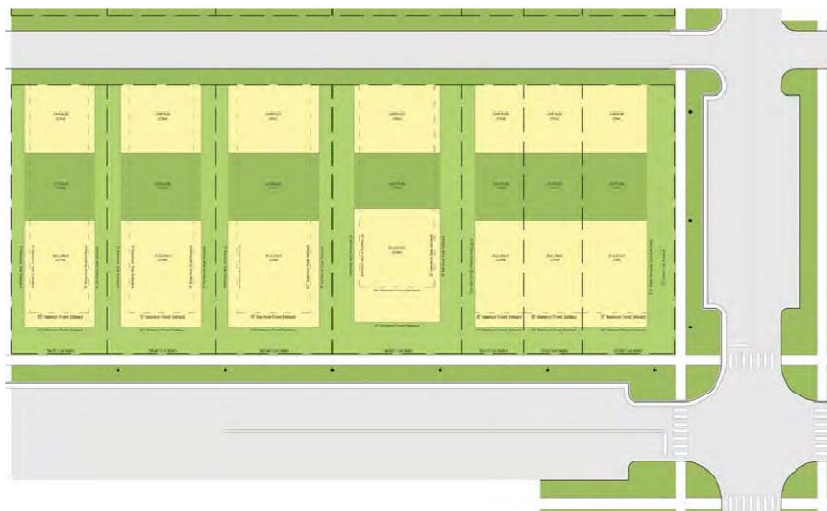
The following examples show potential TND lot sizes and layouts. The lot sizes vary from town home lots to larger single family lots, and include the reduced setbacks and pedestrian orientation that are typical of TND communities. TND projects should include a mix of lot types and sizes within the development, within neighborhoods, and even within individual blocks. Without a carefully mix of residential lot types, the TND form of land use will not be successful.

Final lot types and requirements and designs should be sensitive to existing and projected housing demands and be based on an appropriate TND master plan and Code of Development for each planned TND project.

TND Lots

A prevalent theme in successful American TND models is the use of rear loading for residential lots. Of course, this theme among TND developments is drawn from the prevalence of alleys within successful residential neighborhoods in some of the county's most well-liked old towns. The use of alley access has the advantage of allowing an unbroken streetscape, including eliminating conflicts between pedestrians and cars pulling into and out of driveways. In addition, the alley provides a convenient utility or service access, including a more attractive way to deal with trash pickup.

Most TND communities are designed to include mid-block rear alleys like the ones shown here. These alleys allow access to garages at the rear of residential lots. Additional parking within lots, trash collection, and other needs can also use these alleys. These lot examples show a minimum front setback of 10 feet, and a maximum front setback of 20 feet. Establishing a minimum and maximum setback allows design flexibility and variation, while also maintaining an organized and cohesive look within the neighborhood. Side setbacks shown here are 6 feet for each lot, for a minimum of 12 feet of separation between houses.



Cottage Lot:



Cottage Lot Dimensions:

<i>Lot Width:</i>	<i>34 - 38'</i>
<i>Lot Depth (min.):</i>	<i>80'</i>
<i>Front Yard Setback (min.):</i>	<i>10'</i>
<i>Front Yard Setback (max.):</i>	<i>15'</i>
<i>Side Yard Setbacks (min.):</i>	<i>5'</i>
<i>Side Yard Setbacks (max.):</i>	<i>8'</i>
<i>Rear Yard Setback:</i>	<i>15'</i>

Village Lot:



Village Lot Dimensions:

<i>Lot Width:</i>	<i>38 - 42'</i>
<i>Lot Depth (min.):</i>	<i>85'</i>
<i>Front Yard Setback (min.):</i>	<i>10'</i>
<i>Front Yard Setback (max.):</i>	<i>15'</i>
<i>Side Yard Setbacks (min.):</i>	<i>5'</i>
<i>Side Yard Setbacks (max.):</i>	<i>9'</i>
<i>Rear Yard Setback:</i>	<i>15'</i>

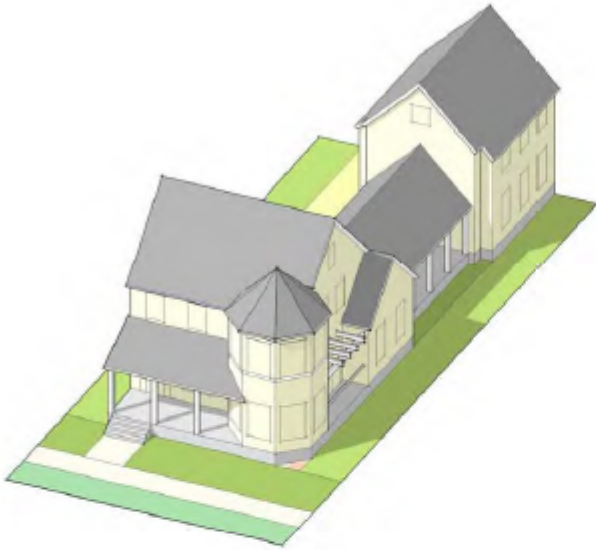
Neighborhood Lot:



Neighborhood Lot Dimensions:

<i>Lot Width:</i>	<i>42 - 60'</i>
<i>Lot Depth (min.):</i>	<i>90'</i>
<i>Front Yard Setback (min.):</i>	<i>10'</i>
<i>Front Yard Setback (max.):</i>	<i>25'</i>
<i>Side Yard Setbacks (min.):</i>	<i>5'</i>
<i>Side Yard Setbacks (max.):</i>	<i>12'</i>
<i>Rear Yard Setback:</i>	<i>15'</i>

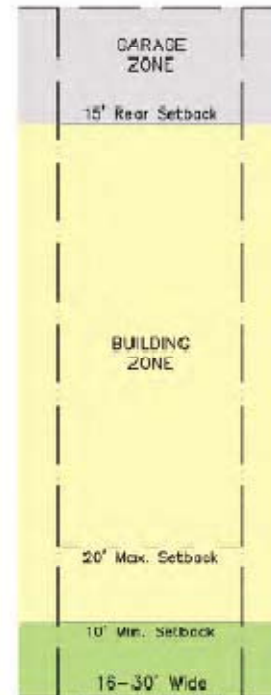
Suburban Lot:



Suburban Lot Dimensions:

<i>Lot Width:</i>	<i>60 - 100'</i>
<i>Lot Depth (min.):</i>	<i>100'</i>
<i>Front Yard Setback (min.):</i>	<i>15'</i>
<i>Front Yard Setback (max.):</i>	<i>30'</i>
<i>Side Yard Setbacks (min.):</i>	<i>10'</i>
<i>Side Yard Setbacks (max.):</i>	<i>15'</i>
<i>Rear Yard Setback:</i>	<i>15'</i>

Townhouse Lot:



Townhouse Lot Dimensions:

<i>Lot Width:</i>	<i>16 - 30'</i>
<i>Lot Depth (min.):</i>	<i>90'</i>
<i>Front Yard Setback (min.):</i>	<i>10'</i>
<i>Front Yard Setback (max.):</i>	<i>20'</i>
<i>Side Yard Setbacks (end unit - min.):</i>	<i>5'</i>
<i>Side Yard Setbacks (end unit - max.):</i>	<i>12'</i>
<i>Rear Yard Setback:</i>	<i>15'</i>

Live-Work Lot:



Live-Work Lot Dimensions:

<i>Lot Width:</i>	<i>18 - 32'</i>
<i>Lot Depth (min.):</i>	<i>85'</i>
<i>Front Yard Setback (min.):</i>	<i>5'</i>
<i>Front Yard Setback (max.):</i>	<i>15'</i>
<i>Side Yard Setbacks (end unit - min.):</i>	<i>5'</i>
<i>Side Yard Setbacks (end unit - max.):</i>	<i>12'</i>
<i>Rear Yard Setback:</i>	<i>15'</i>

Front Access Lots

While residential lots served by alleys are predominant in well-planned TND communities, lots with vehicle access from main residential streets are still possible within TND principles, and may be necessary based on market demand, or on physical site features like topography. Even in cases where vehicle access must come from a street rather than an alley, steps should be taken to preserve the walkability and density of the neighborhood by sharing driveways, using side yard driveways to access garages behind the home, or for corner lots, accessing parking or a garage from the more minor of the two fronting streets.



TND STREET FEATURES

The streets of Traditional Neighborhood Developments are necessarily different from the streets in conventional suburban developments. Because the density encourages walking, biking, and general community activity, the streets of the TND are designed more completely than those of the conventional suburb. The concept of “complete streets” should focus on the following major objectives:

- Continuity of street design throughout the community
- A hierarchy of street scale to emphasize important connections or areas
- Connectivity of pedestrian and vehicular infrastructure
- Standardized landscaping as an aesthetic and functional element of the street
- Building frontage guidelines to create an ordered and uniform street wall

Over the past several generations, suburban residential streets have featured abnormally wide pavements and broad, clear shoulders, a condition that promotes high speeds. In order to make TND streets safe for pedestrians and cyclists, the speed of cars is slowed somewhat. Additional features are also added to the street to increase the safety and comfort of pedestrians, as, well as to make for a more visually interesting environment.

Narrow lanes and streets

By narrowing driving lanes, the TND street slows drivers to speeds that are safer for cars, pedestrians, and cyclists alike. Due to the increased density of TND communities, slower speeds do not necessarily mean longer trips, as destinations are much closer. In many suburban areas, lane widths are a minimum of 16 feet, while TND streets typically specify lanes of 12 feet or less. The grid, or network, street pattern typical of TNDs also divides traffic between multiple small roads, rather than combining traffic onto multilane collector roads as in suburban locations, meaning fewer lanes for pedestrians to cross and generally safer conditions. Traffic calming techniques, including speed bumps or bulb-outs at intersections, can further slow traffic and protect pedestrians and cyclists.

Sidewalks and crosswalks

The key feature of TND streets is that they should be designed for multiple users, not solely for drivers. The streets within a TND community should all have sidewalks, almost always on both side of the street. Sidewalks are often four to five feet wide in residential areas, and separated from the street by a planting zone. This separation gives the sidewalks a safer feel, removed from moving cars. Given their importance in supplementing the civic spaces in the community, commercial street sidewalks must be much wider to accommodate busier uses, and may also provide for café space. Crosswalks must be clearly marked within the street. The use of contrasting materials, or hardscaping, such as brick or stone can make crosswalks stand out, as well as signal to drivers to slow for pedestrians.

Street trees

The presence of evenly spaced trees along a street creates a sense of enclosure that slows traffic, while also providing shade to pedestrians in warm climates, and making for a generally more attractive street environment. On residential streets, trees are commonly planted in a four to eight foot planting zone between the street curb and sidewalk. For commercial streets, trees may be planted in planting beds, or may be installed in tree grates to create additional sidewalk space.

On street parking

In contrast to typical suburban construction that includes both roads and large parking lots on individual commercial parcels, TND streets are designed to include on-street parking. This parking arrangement works toward the TND community's land use goals, as well as its goals for multi-use streets. By parking within the street rather than on individual parcels, the TND can reach much higher densities. In addition, a row of parked cars forms a buffer between moving traffic and pedestrians to give sidewalks a safer and more pleasant feel.

Buildings close to the street

Instead of the minimum setback lines established by traditional zoning codes to ensure that buildings aren't built too close to the street, TND communities are often governed by build-to lines to ensure that structures aren't built too far back from the street. Shallow front setbacks help TNDs achieve their goals of higher density, as well as their street design goals. While in a car-only suburb, buildings near the street might block sight lines and slow traffic, TNDs desire slower traffic, as well as convenience for pedestrians. With closely set buildings and on-street parking, pedestrians do not have to cross parking lots to reach the fronts of buildings as they would in suburban settings.

Street furniture

An additional enhancement of TND streets over traditional suburban streets is the provision of street furniture. Street furniture includes benches, bicycle racks, bollards, planters, and other accessories placed on or near TND streets and sidewalks for the convenience of non-automobile travelers. At the same time, features not friendly to pedestrians and cyclists, such as trash pickup are often handled in alleys or other off-street locations.

TND STREETS

Because of their narrower pavement width, on-street parking, and other pedestrian amenities, Traditional Neighborhood streets are necessarily different in section and overall design from typical suburban feeder or collector streets.

The following examples show five types of TND streets, for use within appropriate areas of master planned TND communities. These examples illustrate potential street sections that include travel lanes, landscape areas, and sidewalks within a public right of way.





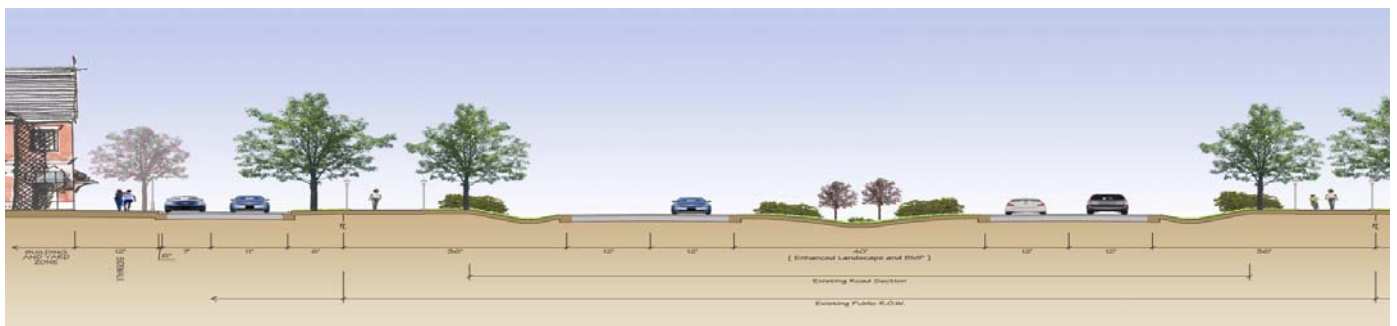
Thoroughfare

A constant question when it comes to TND development is how to address major, existing transportation corridors like Route 17 in Gloucester Point. The same traffic calming measures that are applied to new TND streets often can't be applied to these roads due to state highway requirements or to the importance of these routes to the larger community and region.

However, improvements can be made to major roads to make them more functional within TND zones, including pedestrian and landscape upgrades, to turn a collector road into a boulevard. Pedestrian access can still be provided along major routes by implementing walking and biking trails sufficiently removed from the roadway. These paths should be wider than neighborhood sidewalks, and allow for short walking trips, as well as longer bicycle trips, including commuting. Landscaping can be added to major routes in the form of uniformly spaced street trees, as well as screening plants along pedestrian paths and right of way edges.

Over time, some major routes can be transformed from high-volume arteries to community thoroughfares in order to compliment new, mixed use development in the UDA.

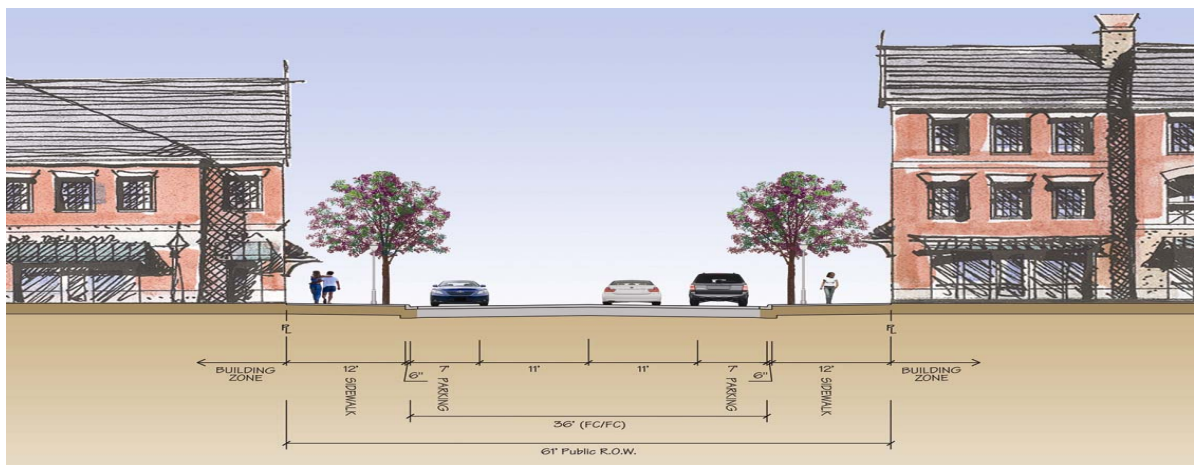
Movement:	High-volume, two way traffic, divided highway
Intersections:	Signalized intersections
Location:	Existing major collectors and regional transportation routes connecting towns and significant developments
Speed:	35 - 45 mph
Lanes:	Multiple lanes in each direction, variable widths, often 12' +
Right of Way:	Existing, variable
Parking:	No on-street parking
Sidewalks:	Sidewalks or multi-use trails, variable width
Paved Edge:	Ditch section (rural and suburban areas) Curb and gutter (urban areas and new construction)
Landscaping:	Street trees and screening
Hardscaping:	Delineated crosswalks, street lights



Village Center Street

One of the most important features of Traditional Neighborhood Design is that developments in this style have a clearly defined center. This center is often made up of a mixed use street or crossroads with retail, office, and residential uses in multi-story buildings, not unlike Main Street development in traditional Virginia towns. This street design provides a framework for such a mixed-use arrangement by being as much a street for pedestrians as for cars. The wide, shaded walks shown here support retail shopping and an active pedestrian environment. Street furnishings should be provided, in the form of benches, streetlights, and other amenities, and buildings should be built at the sidewalk's edge, with setbacks granted for café seating or civic gathering spaces. Ample parking is provided at the curb, and may be supplemented by additional off-street parking.

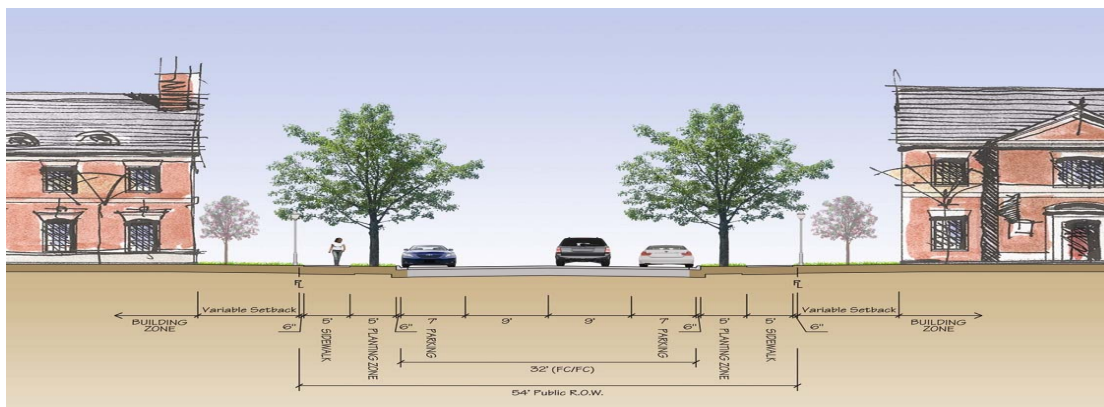
Movement:	Two way traffic
Intersections:	Signalized major intersections Unsignalized minor intersections
Location:	TND core areas with commercial and mixed uses
Speed:	25 mph max
Lanes:	One lane in each direction, 11' lanes
Right of Way:	61' (variable per conditions: 58'-70')
Parking:	On street parallel parking, both sides of street
Sidewalks:	12', both sides of street
Paved Edge:	Curb, or curb and gutter; underground drainage
Landscaping:	Street trees in grates, approx. every 40'
Hardscaping:	Delineated crosswalks, street lights, bollards



Residential Street

Residential streets within Traditional Neighborhood Developments must provide convenient vehicle access to neighborhoods, but must also be conducive to a quiet and safe neighborhood character. This street design can be used as the primary public street within TND residential neighborhoods. It serves as the neighborhood's spine, providing access to residential blocks and connections to the mixed-use core and external roads. The section design is compact, with narrow lanes that slow vehicle speeds to create a quieter neighborhood environment that is safer for pedestrians. This street section design includes sidewalks on both sides, separated from on-street parking and travel lanes by landscape and tree planting zones.

Movement:	Two way traffic
Intersections:	Unsignalized intersections
Location:	TND residential and transitional areas with residential uses
Speed:	25 mph max
Lanes:	One lane in each direction, 9' lanes
Right of Way:	54'
Parking:	On-street parallel parking, both sides of street
Sidewalks:	5', both sides of street
Paved Edge:	Curb, or curb and gutter; underground drainage
Landscaping:	5' planting strip behind curb Street trees approx. every 40'
Hardscaping:	Delineated crosswalks



One Way Street

Within the TND, a one way street is sometimes desirable, whether it is necessary to save space, improve traffic organization, or increase pedestrian safety. It is best though, that one way streets be used sparingly, and only for short distances, as they make it harder for vehicles to navigate, especially for those unfamiliar with the area. When a one way street is used, landscape quality and pedestrian amenities should not be sacrificed, even though the street might be of minor importance to cars.

Movement: One way traffic

Intersections: Unsignalized intersections

Location: TND residential, commercial, and transitional areas with short blocks length, and low expected traffic volumes

Speed: 25 mph max

Lanes: One lane only, 13' lane width

Right of Way: 42'

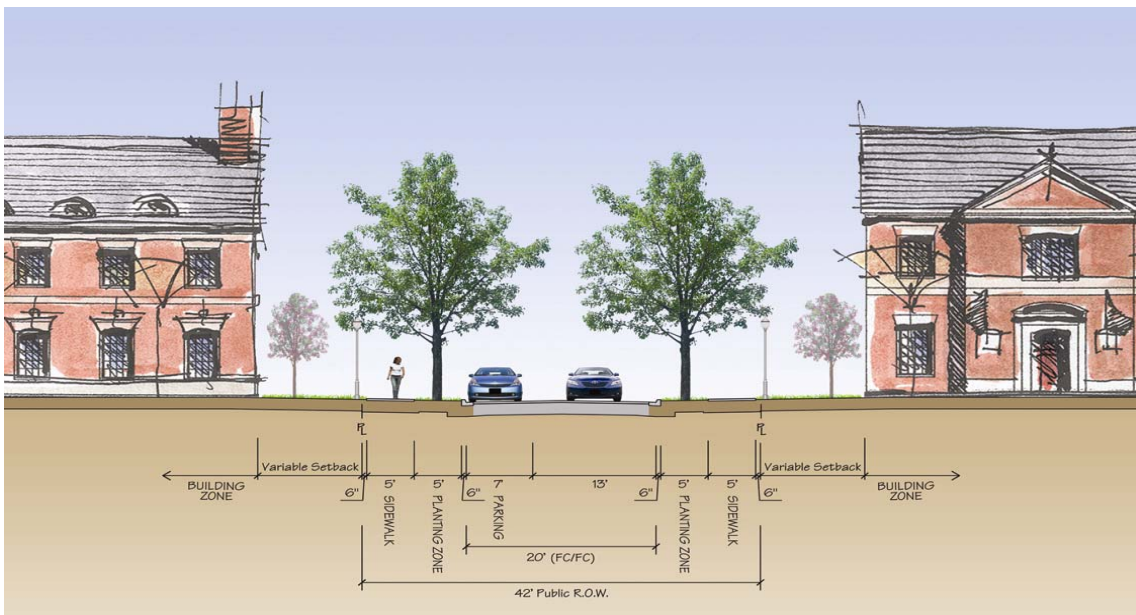
Parking: On-street parallel parking, one side of street

Sidewalks: 5', both sides of street

Paved Edge: Curb and gutter (urban areas and new construction)

Landscaping: Street trees and screening

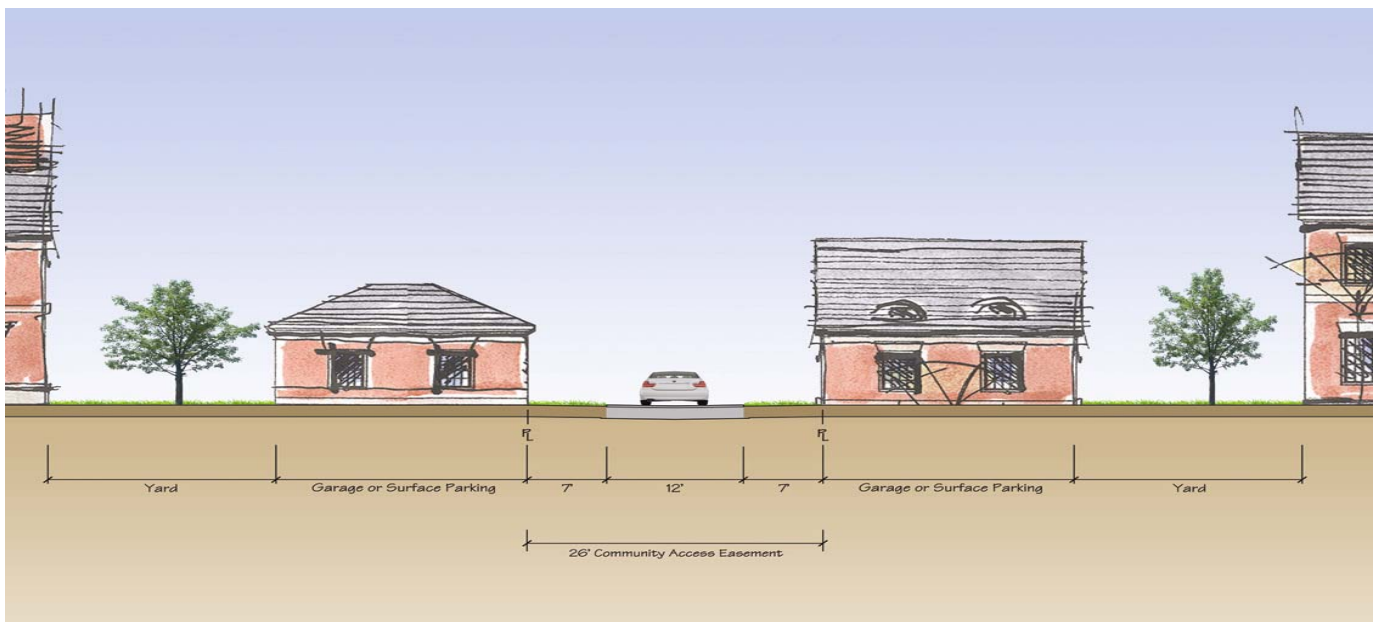
Hardscaping: Delineated crosswalks



Alley

The goal of the TND street is to add amenities that increase the street's usefulness to all street users while removing certain street elements that obstruct non-car street users. The use of alleys helps to relocate some street elements that obstruct pedestrians, yet are necessary to the function of the neighborhood. Alleys are narrow, very low volume vehicular paths located mid-block to provide access to the rear of TND homes or businesses. In TND construction, garages are located on the alley, as is any off-street residential parking. Alleys may also be used for trash pickup, utility service corridors, or occasional loading zones. The alley may be maintained as a private street under the authority of a home owners association, or in some circumstances, dedicated as public access. With utilitarian functions moved off of the main street, the usefulness and attractiveness of the neighborhood street is enhanced.

Movement:	Very low volume, two way traffic
Intersections:	Un-signalized
Location:	Limited, mid-block for rear garage or service access
Speed:	5 mph optimal
Lanes:	Single, unmarked 12' travel way
Right of Way:	Private access easement, variable width
Parking:	No on-street parking
Sidewalks:	No sidewalks required
Paved Edge:	No curb
Landscaping:	None required
Hardscaping:	None required



Other TND Features

Pedestrian Improvements

Any development within an Urban Development Area should function as a pedestrian-friendly environment favoring Traditional Neighborhood Design precepts in which residents have convenient walking access within the development, as well as access to amenities, goods, and services in other parts of the community. Any TND development proposal should include standards the location and design of all sidewalks and trails necessary for complete pedestrian accessibility throughout the community

Sidewalks should be provided along all public streets within the UDAs. Sidewalk locations, as well as design and construction details, should meet all state and local requirements for adoption into the public street system, and should also meet ADA requirements where applicable. Sidewalks should generally be at least 3 feet in width, with wider sidewalks necessary in the most dense and active areas of TNDs. Crosswalks should also be included, and should be well marked.

Pedestrian trails can be provided within public spaces and common areas, forming a more passive off-street circulation system connected to the traditional sidewalks located in the public street rights-of-way. Whereas sidewalks follow the streets and are built of durable materials, paths might curve throughout parks and open space and be built of gravel or other natural materials.



Fences and Walls

In general, Traditional Neighborhood Development rules should approach fencing with broad flexibility. The design of fences for residential neighborhoods should be coordinated to the extent possible to maintain an ordered look to neighborhoods. Fences should also be built of quality, durable materials. Walls, whether for privacy or as a landscape retaining wall, should be built of quality, attractive materials, and designed with the overall look and feel of the neighborhood in mind.

Benches and Other Hardscape Improvements

Benches, tables and other outdoor furnishings are an amenity to pedestrians in any TND, and should be provided in public parks and civic spaces. The style of these should complement the overall look of the neighborhood. Trash receptacles should be provided in parks and other places where people are expected to gather.

Building Form and Massing

The objective of architectural design for a TND project should be to ensure a continuity and compatibility amongst all architectural elements and to maintain a high level of quality and design excellence. While it is not the intent to legislate taste, building forms in a TND community should achieve compatibility in scale, visual order, rhythm, and proportion. The scale of building elements (roofs, doors, windows, porches, columns) should be chosen with the pedestrian in mind and should be proportioned to building's height and volume. Visual order is achieved through a consistent use of these elements in individual buildings. The coordinated repetition and massing of building forms and architectural elements achieves a proper rhythm of neighborhood buildings.

Landscape Design

One of the distinguishing features of Traditional Neighborhood Development is the coordination of streets, buildings, and landscape into a cohesive whole, rather than a group of disparate parts as in typical suburban development. The coordination of these elements is essential to the livability and vitality of Virginia's most attractive towns and cities, and is sorely missing from others. Landscape forms associated with streetscapes, buffer areas, and other landscape zones will be important ingredients in the success of UDAs as ordered village areas, rather than merely intensive subdivisions.

This section establishes landscape expectations for projects within Urban Development Areas, including design concepts, types of plants, and maintenance standards

Grading and Building Sites

The compactness of Traditional Neighborhood Design, including its emphasis on pedestrian activity and buildings sited closer to streets, means that relationships between buildings and landscape are much more obvious than in other types of development. Landscape improvements within the UDA's major development categories; Village Core areas, Transitional areas, and Residential areas, should focus on complementary landscape forms, patterns, and materials to create an ordered appearance.

In general, it is desirable for residential dwellings to be slightly elevated above public streets to improve aesthetics, visibility, and site drainage. Where buildings are lower than street level, special landscape and hardscape attention should be given to the street's edge with private lawns, creative sidewalk placements, or civic spaces.

While not possible in every case, buildings within the same neighborhood grouping or cluster should have coordinated finished floor elevations and/or massing and details which

relate the individual buildings to each other. Berms, when they are used, should be gently curved and rolling at an appropriate scale. Artificial-looking earth mounds are not recommended.

To reduce potential environmental impacts, site designs should minimize the extent of mass grading within the community. All site grading should be done in a way that preserves existing trees and vegetation when possible. A maximum slope of 3:1 around exterior building walls should be maintained, provided that retaining walls may also be necessary in some instances to preserve existing vegetation and to create an attractive landscape.

Existing natural areas, passive open spaces, and pocket parks situated outside building zones should be treated as parts of a community-wide system of open spaces to enhance the visual character of the community.

Plant Materials

The following standards should apply to plants, trees, and other landscape materials in order to create the desired visual harmony among the individual development areas within UDAs. All plant material should be selected from varieties that are native to Virginia whenever possible.

All streets within TNDs should have a regular pattern of street trees for aesthetic value, and to shade sidewalks. Street trees should generally be placed 40 to 60 feet apart, and planted in a landscape zone provided as part of the street section design, or in large, permanent planters or tree grates as necessary.

Ornamental trees should be provided in residential yards, parks, and in landscaped areas of the Village Core for accent and visual emphasis.

Evergreen trees should be used at strategic locations for screening and buffering due to their dense foliage, but also incorporated into landscaping in parks and civic spaces to enhance aesthetics during winter.

Deciduous shrubs should be used as accents on private residential lots, as well as in parks, commercial areas, and other community spaces. Shrubs can be used for visual interest, as well as for screening of items like utility meters and HVAC equipment.

Areas of grass and ground cover can be used in a variety of applications, including hillsides, as a way to control erosion and runoff, and park spaces for recreational use. Care should be taken to use grasses that require as little maintenance as possible.

Landscape Maintenance

Property Owners' Associations or Business Associations should be used to maintain streetscape and ornamental landscaping within condominium-type residential and business districts. Where parks or other open space are dedicated to public use, these

spaces should be publicly maintained. Landscaping within other residential areas will be maintained by individual lot owners. In all cases, proper maintenance should be encouraged, with neighborhood covenants as necessary, to maintain an attractive and vibrant community life. By carefully choosing plant materials, including native species, at the outset, some maintenance effort can be saved.

Stormwater Management and Best Management Practices

A TND rezoning application should contain documentation satisfactory to the County that a system of best management practices (BMPs) and stormwater management (SWM) improvements can be implemented. Rather than constructing SWM/BMP facilities on a lot-by-lot basis, a more regional, comprehensive approach should be developed to address the stormwater runoff of the TND and its watershed. The environmental goal should be to make downstream runoff conditions better than current conditions. The use of contemporary Low Impact Development improvements is highly recommended.

In conjunction with a rezoning application, the applicant should be required to prepare a comprehensive Stormwater Management Master Plan in coordination with the County addressing the contributing drainage sheds intersecting the property. The study would include preparation of preliminary hydraulic and hydrologic engineering designs for an integrated system of on-site stormwater management facilities to further establish the feasibility of the stormwater or floodplain improvements as depicted on the master plan.

Where TND projects incorporate floodplains and stream valleys, the master plan should integrate the design for the stormwater management facilities into a comprehensive stream valley open space, parks, and trails system that benefits the entire community. In addition, the applicant should be required to develop a program for the perpetual maintenance of on-site stormwater management facilities, floodplain areas, and related improvements that is acceptable to the County.

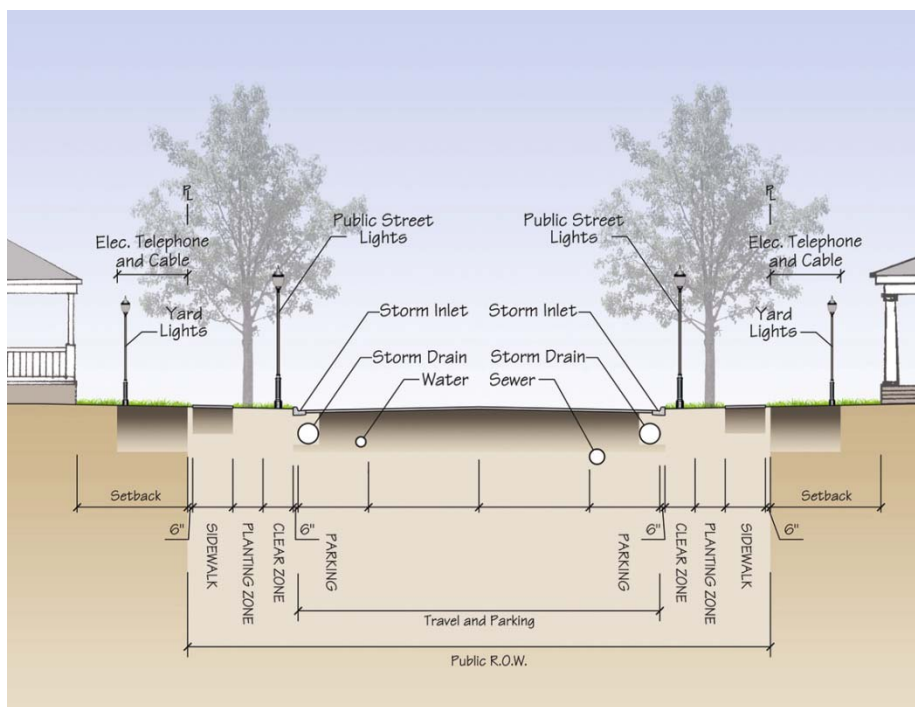
From an environmental management standpoint, SWM and BMP facilities should be protected and maintained during the construction process, and, upon completion, they should be maintained by either a property owners' association or the County. It should be the responsibility of individual lot developers to provide this protection during construction and to ensure that both SWM and BMP capacity is maintained throughout. Where LID and BMP facilities are to be constructed on private lots (and which are not otherwise publicly maintained), the maintenance should be the responsibility of the individual lot owner.

Utilities

The goals for compact development set by the Urban Development Area legislation cannot be reached without the provision of similarly compact public water, sewer, drainage and other essential utilities. Highly coordinated designs for public utility systems require less

space than would otherwise be consumed by conventional easements. Further, they allow homes to be placed on much smaller lots than would be necessary if they were to be served by only on-site well and septic systems. Traditional Neighborhood Design densities and street section configurations also mean that stormwater runoff will often be handled by a curb and gutter setup with underground piping to convey collected surface runoff, rather than by ditches. In addition to water, sewer, and stormwater, other infrastructure such as gas, electrical, communications, and television cable are typically provided in TND construction.

Utilities should primarily be located within public street rights of way in order to allow full use of buildable areas, facilitating the desired shallow setbacks typical of TND construction. In addition to these rights of way, utility easements may also be established within the rear alleys that will serve many TND neighborhoods. To achieve this goal, easements of reduced width as well as shared easements are appropriate for this type of neighborhood. In certain cases, common, shared, or overlapping easements for water, sewer, and drainage improvements will be needed to achieve the desired street sections and building lot placements. County officials will likely need to lead in the coordination effort, given the suburban mentality of many local utility agencies to protect their own easement territory.



Appendix C: Public Outreach & Involvement

One of the reasons for selecting the Gloucester Point/Hayes area as the portion of the County for consideration during this planning effort was because of the input received during Gloucester Point Supervisor Crewe's bi-monthly town hall meetings held at Abingdon Elementary School. The second Tuesday of each month, residents, business owners, and County staff joined Supervisor Crewe to talk about the status of efforts that impacted area. Based on staff's attendance, there was knowledge of the local resident and business concerns for the area based on the Route 17 widening project and what its impact would be- not only on the traffic, but on the economy and way of life in and around Gloucester Point. The following listing provides an overview of the actions taken during the progress of the Gloucester Point/Hayes Village Development Area plan amendment to gather input from the community and keep them informed from beginning to end. In the pages after the listing, more details have been provided.

Board of Supervisors Authorizes Acceptance of VDOT Grant and Agreement	April 20, 2010
Board of Supervisors Authorizes County Administrator Established Steering Committee	September 7, 2010
Online/Phone Survey to Gather Community Input- Nearly 300 responses!	October-November 2010
"Kick Off" Meeting at Abingdon Elementary School- Hosted by Supervisor Crewe (over 80 people in attendance)	November 9, 2010
Steering Committee Forms & Meets Monthly	December 2010-Present
Presentation to the Economic Development Authority (EDA)- Gloucester Point Subcommittee	April 15, 2011
Presentation to the EDA	April 26, 2011
Updates to Planning Commission	Monthly (July 2010-Present)
Update to Board of Supervisors	August 2, 2011
Community Outreach Meeting at Abingdon Elementary School- Hosted by Supervisor Crewe (over 75 people in attendance)	August 9, 2011
Planning Commission Public Hearing	September 1, 2011
Board of Supervisors Public Hearing	

AT A REGULAR MEETING OF THE GLOUCESTER COUNTY BOARD OF SUPERVISORS, HELD ON TUESDAY, APRIL 20, 2010 AT 7:00 P.M., IN THE BOARD ROOM IN THE COLONIAL COURTHOUSE, COURT CIRCLE, 6504 MAIN STREET, GLOUCESTER, VIRGINIA: ON A MOTION DULY MADE BY MR. RILEE, AND SECONDED BY MR. BORDEN, THE FOLLOWING RESOLUTION WAS ADOPTED BY THE FOLLOWING VOTE:

Carter M. Borden, yes;
Robert A. Crewe, yes;
John H. Northstein, yes;
Michelle R. Ressler, yes;
Christian D. Rilee, yes;
Louise D. Theberge, yes;
Gregory Woodard, absent;

**RESOLUTION AUTHORIZING THE COUNTY ADMINISTRATOR
TO EXECUTE AN URBAN DEVELOPMENT GRANT PROGRAM
AGREEMENT**

WHEREAS, in 2007, the General Assembly added Section 15.2-2223.1 to the Code of Virginia requiring high growth localities, such as Gloucester County, to designate Urban Development Areas (UDA's) in their Comprehensive Plans by 2011;

WHEREAS, UDA's are areas of compact development that can accommodate 10 to 20 years of projected growth and incorporate the principles of new urbanism and traditional neighborhood design (TND) such as mixed uses and pedestrian friendly streets; and

WHEREAS, the Board of Supervisors, the Planning Commission, and Comprehensive Plan Steering Committee are aware of the requirement to designate UDA's in the Comprehensive Plan and the Steering Committee has discussed UDA designations in Gloucester and will be providing recommendations regarding their designation in their recommendations for the Comprehensive Plan update; and

WHEREAS, in October 2009, Commonwealth of Virginia, Office of Intermodal Planning and Investment and the Virginia Department of Transportation (VDOT) announced grant opportunities to help localities comply with Section 15.2-2223.1 by providing funding for the use of VDOT's on-call consultants; and

WHEREAS, in addition to helping localities analyze growth patterns and identify appropriate UDA location(s), the grant also provides for creating sub-area plans and revising local land use ordinances to incorporate the principles of new urbanism and TND for implementation of the UDA's by June 30, 2011; and

WHEREAS, staff applied for and received the grant in the amount of \$50,000 which requires no local match; and

WHEREAS, in order to initiate the use of the funds provided by the grant, the County must enter into an agreement with the Commonwealth of Virginia, Office of Intermodal Planning and Investment.

NOW, THEREFORE BE IT RESOLVED by the Gloucester County Board of Supervisors that the County Administrator is authorized to execute the Urban Development Area Grant Program Agreement; and

BE IT FURTHER RESOLVED that the County Administrator be, and hereby is, authorized to take all actions and sign any contracts or other documents necessary as required by this grant program to accept the grant award.

BI IT STILL FURTHER RESOLVED that the \$50,000 received by this grant be, and hereby is, appropriated to the Planning Department for the fiscal year beginning July 1, 2010 to June 30, 2011 as the date for expenses related to this grant.

A Copy Teste:



Brenda G. Garton, County Administrator

AT A REGULAR MEETING OF THE GLOUCESTER COUNTY BOARD OF SUPERVISORS, HELD ON TUESDAY, SEPTEMBER 7, 2010 AT 7:00 P.M., IN THE COLONIAL COURTHOUSE, COURT CIRCLE, 6504 MAIN STREET, GLOUCESTER, VIRGINIA: ON A MOTION DULY MADE BY MR. CREWE, AND SECONDED BY MR. NORTHSTEIN, THE FOLLOWING RESOLUTION WAS ADOPTED BY THE FOLLOWING VOTE:

Carter M. Borden, yes;
Robert A. Crewe, yes;
John H. Northstein, yes;
Michelle R. Ressler, yes;
Christian D. Rilee, yes;
Louise D. Theberge, yes;
Gregory Woodard, yes;

**RESOLUTION AUTHORIZING THE COUNTY ADMINISTRATOR
TO ESTABLISH A GLOUCESTER POINT URBAN
DEVELOPMENT AREA (UDA) STEERING COMMITTEE**

WHEREAS, in 2007, the General Assembly added Section 15.2-2223.1 to the Code of Virginia requiring high growth localities, such as Gloucester County, to designate Urban Development Areas (UDA's) in their Comprehensive Plans by 2011;

WHEREAS, UDA's are areas of compact development that can accommodate 10 to 20 years of projected growth and incorporate the principles of new urbanism and traditional neighborhood design (TND) such as mixed uses and pedestrian friendly streets; and

WHEREAS, the Board of Supervisors, the Planning Commission, and Comprehensive Plan Steering Committee are aware of the requirement to designate UDA's in the Comprehensive Plan and the Steering Committee has discussed UDA designations in Gloucester and will be providing recommendations regarding their designation in their recommendations for the Comprehensive Plan update; and

WHEREAS, staff applied for and received a grant from Commonwealth of Virginia, Office of Intermodal Planning and Investment and the Virginia Department of Transportation (VDOT) in the amount of \$50,000 and on April 20, 2010 the Board of Supervisors authorized the County Administrator to execute the Urban Development Area Grant Program Agreement with the Commonwealth of Virginia, Office of Intermodal Planning and Investment; and

WHEREAS, the Planning Department received a notice to proceed from the Office of Multimodal Planning on June 24, 2010 with a required completion date of September 30, 2011; and

WHEREAS, the Comprehensive Plan Steering Committee has identified both the Court House area and the Gloucester Point area as potential UDA's for the County; and

WHEREAS, due to the concentration of population and the mix use elements currently in place in the Gloucester Point region of the County and in light of the recent planning efforts that resulted in the *Gloucester Courthouse Village Plan*, it was determined that Gloucester Point would be the initial UDA study area for the grant, with the Court House area being a potential second UDA depending on the results of the study; and

WHEREAS, in preparing for the public engagement process staff consulted with representatives from the Economic Development Authority's Gloucester Point Committee to determine how to best engage the public; and

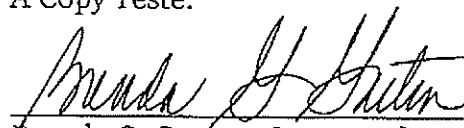
WHEREAS, it was determined that a steering committee made up of key stakeholders in the Gloucester Point community working with staff from the Planning Department, Department of Economic Development and other appropriate Departments would be the best method to oversee the planning process which would also involve broader community outreach through surveys, town meetings and individual stakeholder interviews, and

WHEREAS, the steering committee will provide input and recommendations on development of the plan and code amendments by The Cox Company which will then be forwarded to the Planning Commission for their review and recommendation to the Board of Supervisors for adoption.

NOW, THEREFORE BE IT RESOLVED by the Gloucester County Board of Supervisors that the County Administrator is authorized to establish a Gloucester Point Urban Development Area (UDA) Steering Committee consisting of approximately 10 people representing Gloucester Point area business owners, residents and civic groups; the development and homebuilding community, the real estate industry, the Virginia Institute of Marine Sciences, Gloucester County Economic Development Authority, and a Gloucester Point area representative from the Board of Supervisors, the Planning Commission and the Comprehensive Plan Steering Committee; and

BE IT FURTHER RESOLVED that the Planning Department will serve as the primary staff support for the Gloucester Point UDA Steering Committee.

A Copy Teste:



Brenda G. Garton, County Administrator



Gloucester Department of Community Education

P.O. BOX 1306

GLOUCESTER, VIRGINIA 23061

804-693-5730 FAX: 693-0509

EMAIL: clewis@gloucesterva.info

FOR IMMEDIATE RELEASE

Contact: Christi Lewis, Community Education

693-5730 or clewis@gloucesterva.info

September 9, 2010

Gloucester Point Representatives Needed for Urban Development Area Steering Committee

The County of Gloucester is establishing an Urban Development Area Steering Committee to work with county staff and consultants to oversee the planning process to designate and plan for an Urban Development Area (UDA) for the Gloucester Point area. UDAs are areas of compact development that accommodate up to 10, but no more than 20, years of the County's projected growth and incorporate the principles of new urbanism and traditional neighborhood design, such as mixed uses and pedestrian friendly streets. The state mandates the designation of at least one UDA as part of Gloucester County's comprehensive plan and funding is provided to accomplish the mandate. The planning process will involve the development of county code amendments that allow for the type of development envisioned in the UDA plan. The project completion date is September 2011. For more information on UDA's, visit the Gloucester County Planning Department's website at www.gloucesterva.info/planning/homepage.htm (Comprehensive Plan Update) or contact the Planning Department at 693-1224.

The steering committee will be composed of 10 or more individuals representing the following areas: Gloucester Point area business owners, residents, community organizations, development/homebuilding, real estate, Virginia Institute of Marine Science, Gloucester Economic Development Authority, as well as a member from the Planning Commission, the Comprehensive

Continued...

Plan Steering Committee and a member of the Gloucester Board of Supervisors. The committee will meet as needed at a place and time to be determined.

Interested citizens should complete a Volunteer Board Bank Interest Form which may be found on the County's website at www.gloucesterva.info. (On the e-services drop down menu select "Boards/Committees On-Line.") Interest forms may also be obtained by contacting the Community Education Department at 693-5730. The deadline for interested candidates is Thursday, September 30, 2010.

For more information concerning the Urban Development Area Steering Committee please call the Gloucester Planning Department at 693-1224.

End of Announcement

An Urban Development Area is an area of the County selected for future higher density, mixed-use growth. The UDA will be included in the County's Comprehensive Plan, where it will be shown on the Future Land Use Map. Areas suitable for a UDA are those that are well-served by existing transportation infrastructure and public utilities.

The State of Virginia has recently mandated that localities with more than 20,000 residents that grew more than 5% between 1990 and 2000 must designate at least one Urban Development Area in their comprehensive plan. Over this ten year period, Gloucester County grew by 15%.

During the recent Comprehensive Plan outreach meetings, the Gloucester Point area was identified as a possible location for a UDA in Gloucester. A generalized map of the area is shown below. The purpose of this survey is to gather input from you and your fellow citizens so that this area can be more specifically defined. Please take a moment to fill out the questions on the following pages:

A hand-drawn map of the coastal area around San Francisco, showing the coastline, major roads, and various landmarks. The map is oriented with North at the top. Key features include the Golden Gate Bridge, the city of San Francisco, and the surrounding hills. The map is drawn on a grid of latitude and longitude lines.

And

I. The Gloucester Point Planning Area

1. The Gloucester Point area is an appropriate area to encourage future residential and commercial development due to its convenient location within the County and proximity to existing services and amenities.

Strongly Disagree **1** **2** **3** **4** **5** *Strongly Agree*

2. New commercial and employment opportunities will stimulate jobs and investment in the County and areas should be designated for their future location in and around Gloucester Point.

Strongly Disagree **1** **2** **3** **4** **5** *Strongly Agree*

3. Great communities often have a strong center. What area do you consider to be the center of Gloucester Point?

4. What are the three most important issues facing the Gloucester Point area in the future (*such as housing choices, employment opportunities, shopping close to home, traffic, energy costs*)?

5. New residences are a priority near Gloucester Point and should be encouraged.

Strongly Disagree **1** **2** **3** **4** **5** *Strongly Agree*

6. In your opinion, what mix of housing types (single family detached, townhomes, multifamily residences) should be developed in the Gloucester Point area? Please provide your ideal mix in terms of percentages of total future dwelling units (*percentages should total 100%*):

Single Family Detached Homes: _____ % Multifamily/Apartment: _____ %

Townhomes: _____ % Total: 100 %

7. What establishments and amenities that you now frequent elsewhere would be desirable in the Gloucester Point area (*such as movies, skating, bowling, sports facilities, health clubs, libraries, churches*)?

II. County Services, Facilities and Resources

8. Streets and roads in the County adequately currently serve the needs of local residents.

Strongly Disagree **1** **2** **3** **4** **5** *Strongly Agree*

9. Affordable workforce housing is needed in the County to serve residents with low and moderate incomes.

Strongly Disagree **1** **2** **3** **4** **5** *Strongly Agree*

10. In order to promote and attract jobs and investment that could eventually increase the County's tax base, I would be willing to support County projects that fund water and sewer projects, storm drainage improvements, and areas for employment opportunities.

Strongly Disagree **1** **2** **3** **4** **5** *Strongly Agree*

11. In order to improve residents' quality of life, I would support County projects that add sidewalks, street lights, civic spaces, and recreation facilities to new and existing residential developments.

Strongly Disagree **1** **2** **3** **4** **5** *Strongly Agree*

12. Please indicate your opinions of the following services and resources in the County:

	Good	Fair	Poor	Don't Know
A. Appearance of roads/streets	_____	_____	_____	_____
B. Appearance of neighborhoods	_____	_____	_____	_____
C. Community safety	_____	_____	_____	_____
D. Interaction among neighbors	_____	_____	_____	_____
E. Traffic flow	_____	_____	_____	_____
F. Road maintenance	_____	_____	_____	_____
G. Variety of stores/services	_____	_____	_____	_____
H. Ease of access to stores/shopping	_____	_____	_____	_____
I. Attractiveness of local businesses	_____	_____	_____	_____
J. Attractiveness of signs	_____	_____	_____	_____
K. Parks and recreation facilities	_____	_____	_____	_____
L. Public school facilities	_____	_____	_____	_____
M. Other: _____	_____	_____	_____	_____

III. Shopping and Lifestyle

13. How many times per month do you visit the following commercial establishments?

	Less than 1	1-2 Times	3-4 Times	5+ Times
Grocery Stores	_____	_____	_____	_____
Sit Down Restaurants	_____	_____	_____	_____
Fast Food Restaurants	_____	_____	_____	_____
Home Improvement Stores	_____	_____	_____	_____
Clothing and Shoe Stores	_____	_____	_____	_____

14. Where do you or your family members most frequently shop for retail goods and services?

	Gloucester Point	Other Gloucester Location	York or Newport News	Williamsburg Area	Other Locale
Groceries	_____	_____	_____	_____	_____
Pharmacies	_____	_____	_____	_____	_____
Automobiles	_____	_____	_____	_____	_____
Furniture	_____	_____	_____	_____	_____
Clothing	_____	_____	_____	_____	_____
Home Improvements	_____	_____	_____	_____	_____
Banking	_____	_____	_____	_____	_____
Movies	_____	_____	_____	_____	_____
Restaurants	_____	_____	_____	_____	_____
Fast Food	_____	_____	_____	_____	_____

15. Energy costs have had a negative impact on my weekly shopping habits (for example: when gas prices are high, you are less willing to travel long distances to do your shopping).

Strongly Disagree 1 2 3 4 5 Strongly Agree

16. It would improve my quality of life to have greater shopping, entertainment, and recreation options in close proximity to my home (say, within 1 to 3 miles).

Strongly Disagree 1 2 3 4 5 Strongly Agree

17. It would improve my quality of life to have more sidewalks, bikeways, and parks in close proximity to my home.

Strongly Disagree **1** **2** **3** **4** **5** *Strongly Agree*

IV. Information About You

18. Do you currently live in Gloucester County? ☐ YES ☐ NO

19. Do you live in or near Gloucester Point? ☐ YES ☐ NO

20. If no, in what area of the County do you live? _____

21. Do you rent or own your place of residence? ☐ Rent ☐ Own

22. What type of residence do you live in? (Please check appropriate box)

Single family home: _____

Townhouse: _____

Mobile home: _____

Apartment/ condo: _____

23. Total number of people living in your home: _____

24. Number of school-age children living in your home:

Elementary _____

Middle _____

High School _____

25. Do you work in Gloucester County? ☐ YES ☐ NO

26. If you do not currently work in Gloucester County, what is the location of your place of employment? _____


27. This survey has been an effective means for expressing my attitudes about planning in the County.

☐ YES ☐ NO

Thank you for your time and opinions. Your participation is important and will help guide decisions related to growth management and land use planning in Gloucester County.

If you have questions, comments, or concerns feel free to email them to: comprehensiveplan@gloucesterva.info or call the Planning Department at 693-1224.

The postcard shown below was mailed by Planning Staff in October 2010 to 3500+ addresses for properties that were in and around the area being considered for the Gloucester Point/Hayes Village Development Area:



Community Planning Project for Gloucester Point

A community planning study is underway to determine a vision for the Gloucester Point area. Take our survey today to have your voice heard and help shape the future of the place where you live, work, and/or play.

Please visit the Planning Department homepage at: www.gloucesterva.info/planning to find the online survey and additional information or call the Planning Department at 693-1224 to take the survey over the phone.

Please take a moment to complete the survey either online or by phone and then be sure to join us for an interactive and informative Community Meeting hosted by Supervisor Crewe at Abingdon Elementary on November 9th at 7:00pm.

Gloucester is required by the state to designate areas of the County that are appropriate for future growth. Using grant funding, Gloucester is working with consultants to ensure that an area is created that meets the State's requirements and best serves the citizens of Gloucester.

We need to hear from you!



Gloucester Department of Community Education

P.O. BOX 1306

GLOUCESTER, VIRGINIA 23061

804-693-5730

FAX: 693-0509

EMAIL: clewis@co.gloucester.va.us

MEDIA RELEASE

FOR IMMEDIATE RELEASE

Date:

Contact: Community Education, 693-5730

GLOUCESTER POINT PLANNING TOPIC OF UPCOMING MEETING

Future planning and the potential of the Gloucester Point area will be the focus of an upcoming community meeting hosted by Gloucester Point District Board of Supervisors member Bobby Crewe. The meeting is scheduled for Tuesday, November 9th beginning at 7 p.m. in the Abingdon Elementary School cafeteria.

Supervisor Crewe will be joined by staff from the planning and economic development offices, to hear and learn more about Urban Development Area (UDA) requirements from county consultants, The Cox Company. UDAs are areas served by existing transportation infrastructure and public utilities that are selected for future higher density, mixed-use growth within a traditional neighborhood, in a pedestrian-friendly environment. The General Assembly has mandated that the county designate at least one UDA and offered a grant and on-call state consultants to comply with the mandate. The Cox Company will provide an overview of the UDA requirements and the planning process with an explanation of how to use the requirements to benefit the Gloucester Point area.

The community meeting is being held to provide community and business members with an opportunity to learn about UDAs and share comments and ideas on planning and development of the southern gateway to Gloucester County over the next 10 to 20 years.

Continued....



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MEDIA RELEASE

FOR IMMEDIATE RELEASE

Date: Friday, January 21, 2011

Contact: Community Education, 693-5730

URBAN DEVELOPMENT AREA TOPIC OF GLOUCESTER POINT MEETING

Information on the future planning of the Gloucester Point area will be the main focus of an upcoming community meeting hosted by Gloucester Point District Board of Supervisors member Bobby Crewe. The meeting is scheduled for Tuesday, February 8th beginning at 7 p.m. in the Abingdon Elementary School cafetorium.

Staff for the Gloucester Planning Department will join Supervisor Crewe to continue discussions and answer questions and concerns about the Gloucester Point planning project currently underway. The General Assembly mandated that Gloucester County designate at least one Urban Development Area (UDA). An Urban Development Area is defined as an area served by existing transportation infrastructure and public utilities that is selected by a locality for future higher density, mixed-use growth within a traditional neighborhood, pedestrian-friendly environment. The county was awarded a grant from the state to work with consultants on the project. In addition, a steering committee composed of citizens, business members and county officials has been appointed to oversee and advise on the planning process.

At the community meeting Planning staff will address current zoning and future development as it relates to the Comprehensive Plan, update to the plan and the zoning ordinance. Staff will clearly

Continued...

define the state's requirements for designation of an UDA, the community's role in the process and the potential benefits for an UDA designation. Time will be provided for questions and comments.

Other topics of importance to Gloucester Point residents will be addressed by Mr. Crewe to include reassessment and the Route 17 road improvement construction at Gloucester Point.

The meeting is open to the public and anyone interested is encouraged to attend and participate. For general information about the meeting, please contact the Gloucester Department of Community Education at 693-5730. For specific information on Urban Development Areas, contact the Planning Department at 693-1224.

-- END OF ANNOUNCEMENT



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804-693-5730
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EMAIL: clewis@co.gloucester.va.us

MEDIA RELEASE

FOR IMMEDIATE RELEASE

Date: Wednesday, March 23, 2011

Contact: Community Education, 693-5730

GLOUCESTER POINT AREA COMMUNITY MEETINGS CONTINUE

Information on the future planning of the Gloucester Point area continues to be the main focus of an upcoming community meeting hosted by Gloucester Point District Board of Supervisors member Bobby Crewe. The meeting is scheduled for Tuesday, April 12th beginning at 7 p.m. in the Abingdon Elementary School cafeteria.

Joining Supervisor Crewe will be staff from the Gloucester Planning Department to present updated information on the Gloucester Point planning process. Mr. Crewe will also update community members on the current budget process, reassessment and begin discussions on ideas for beautification efforts once the Route 17 road improvement construction is completed.

The meeting is open to the public and anyone interested is encouraged to attend and participate. For general information about the meeting, please contact the Gloucester Department of Community Education at 693-5730.

-- END OF ANNOUNCEMENT



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MEDIA RELEASE

FOR IMMEDIATE RELEASE

Date: Tuesday, May 31, 2011

Contact: Community Education, 693-5730

GLOUCESTER POINT AREA COMMUNITY MEETING

Gloucester Point Magisterial District Board of Supervisor representative Bobby Crewe has scheduled another in a series of community meetings for Tuesday, June 14, 2011 at 7 p.m. in the Abingdon Elementary School cafetorium. The meeting is a continuation of previously held meetings that focused on the future of Gloucester Point as well as to address any concerns of Gloucester Point area residents and business owners.

For general information about the meeting, please contact the Gloucester Department of Community Education at 693-5730.

-- END OF ANNOUNCEMENT

The following postcard was mailed in July 2011 in preparation for the August 9th meeting at Abingdon Elementary School. Like the original mail outreach, it was mailed to over 3500 addresses in and around the Gloucester Point/Hayes Village Development Area.

Community Planning for the Gloucester Point/Hayes Corridor



Tuesday, August 9, 2011
7:00pm
Abingdon Elementary School

For more information visit the Planning Department homepage at: www.gloucesterva.info/planning or call our office at 693-1224.

Please join us for an interactive and informative **Community Meeting** hosted by Supervisor Crewe at Abingdon Elementary on August 9th at 7:00pm.

The focus of this meeting will be the Gloucester Point/Hayes Planning Project, which has been underway since last fall. Supervisor Crewe and county staff will update attendees on the project's progress so far as well as the next steps. In addition, there will be opportunity for those in attendance to interact and provide feedback for use in shaping the project prior to consideration by the Gloucester Planning Commission and Board of Supervisors.



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EMAIL: clewis@co.gloucester.va.us

MEDIA RELEASE

FOR IMMEDIATE RELEASE

Date: Monday, July 25, 2011

Contact: Community Education, 693-5730

GLOUCESTER POINT/HAYES AREA COMMUNITY MEETING

Gloucester Point District Board of Supervisor representative Bobby Crewe has scheduled another in a series of community meetings for Tuesday, August 9, 2011 at 7 p.m. in the Abingdon Elementary School cafetorium. This meeting is a continuation of previously held meetings focusing on the future of the Gloucester Point and Hayes areas, as well as address concerns of area residents and business owners.

The focus of this meeting will be the Gloucester Point/Hayes Planning Project, which has been underway since last fall. Supervisor Crewe and county staff will update attendees on the project's progress so far as well as the next steps. In addition, there will be opportunity for those in attendance to interact and provide feedback for use in shaping the project prior to consideration by the Gloucester Planning Commission and Board of Supervisors.

Supervisor Crewe will also address road widening and other VDOT projects, the county assessment process and the sign ordinance issue. For more information about the meeting, contact the Gloucester Department of Community Education at 693-5730.

- END OF ANNOUNCEMENT